



Town Council Approved
December 13, 2007

New Directions for Yucca Valley:
**Old Town Yucca Valley
Specific Plan**



Prepared for:
**Town of Yucca Valley
and the Yucca Valley Community**

Prepared by:
**RBF Consulting's
Urban Design Studio**

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OLD TOWN YUCCA VALLEY SPECIFIC PLAN

SP-03-07

Prepared for:

Town of Yucca Valley and the Yucca Valley Community

Prepared by:

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TABLE OF CONTENTS

Chapter 1: Introduction

1.1	Overview.....	1-1
1.1.1	Location and Setting.....	1-1
1.1.2	Purpose and Intent.....	1-3
1.1.3	Historical Perspective.....	1-4
1.2	The Planning Process.....	1-7
1.2.1	Phase I: Information Gathering.....	1-7
1.2.2	Phase II: Concepts and Strategies.....	1-7
1.2.3	Phase III: Preparation of Specific Plan.....	1-8
1.3	Community Outreach.....	1-8
1.4	Vision Plan.....	1-9
1.5	Organization of the Specific Plan.....	1-14
1.6	Specific Plan Authority and Scope.....	1-15
1.6.1	Authority.....	1-15
1.6.2	Relationship to the General Plan.....	1-15
1.6.3	Relationship to the Zoning Ordinance.....	1-15
1.6.4	Compliance with the California Environmental Quality Act (CEQA).....	1-16

Chapter 2: Existing Conditions

2.1	Existing Land Uses.....	2-1
2.1.1	On-Site Land Uses.....	2-1
2.1.2	Surrounding Land Uses.....	2-5
2.2	Existing Improvements.....	2-5
2.3	Topography.....	2-5
2.4	Circulation and Access.....	2-5
2.4.1	Vehicular.....	2-5
2.4.2	Pedestrian.....	2-5
2.5	Infrastructure and Utilities.....	2-6
2.5.1	Water.....	2-6
2.5.2	Wastewater.....	2-6
2.5.3	Storm Drainage.....	2-6
2.5.4	Electricity.....	2-6
2.5.5	Natural Gas.....	2-6
2.5.6	Communication Systems.....	2-6
2.5.7	Solid Waste.....	2-9
2.6	Seismic.....	2-9
2.7	Environmental Resources.....	2-9
2.7.1	Biological Resources.....	2-9
2.7.2	Cultural Resources.....	2-9

Table of Contents

Chapter 3: Development Plan

3.1	Land Use	3-1
3.1.1	Old Town Mixed-Use.....	3-5
3.1.2	Old Town Highway Commercial.....	3-6
3.1.3	Old Town Commercial/Residential	3-6
3.1.4	Old Town Industrial/Commercial	3-6
3.1.5	Highway Environs Overlay	3-6
3.2	Community Facilities	3-7
3.3	Circulation Plan	3-7
3.3.1	Linkages	3-7
3.3.2	Roadway Network.....	3-7
3.3.3	SR-62 Realignment.....	3-17
3.3.4	Traffic Calming.....	3-18
3.3.5	Pedestrian and Bicycle/Equestrian Trails.....	3-18
3.4	Infrastructure Plan	3-18
3.4.1	Domestic Water System.....	3-19
3.4.2	Wastewater System	3-24
3.4.3	Storm Drainage System	3-24
3.5	Public Services	3-24
3.5.1	Schools	3-24
3.5.2	Fire Protection.....	3-27
3.5.3	Law Enforcement	3-27
3.5.4	Library	3-27
3.5.5	Communication Systems.....	3-27
3.5.6	Electricity.....	3-27
3.5.7	Natural Gas.....	3-27
3.5.8	Solid Waste.....	3-27

Chapter 4: Development Regulations

4.0	Purpose and Intent	4-1
4.0.1	Organization of This Chapter.....	4-1
4.1	General Provisions	4-2
4.1.1	Minimum Requirements	4-2
4.1.2	Applicability of Development Standards and Guidelines.....	4-2
4.1.3	Interpretation, Administration, Enforcement, and Appeals.....	4-2
4.2	Establishment of Land Use Districts	4-2
4.2.1	Purposes of Land Use Districts	4-2
4.3	Allowable Land Uses and Permit Requirements	4-3
4.3.1	Allowed Uses.....	4-3
4.4	Nonconforming Uses, Structures, and Parcels.....	4-15
4.4.1	Purpose and Intent	4-15
4.4.2	Nonconforming Uses.....	4-15
4.4.3	Nonconforming Structures.....	4-16
4.4.4	Nonconforming Parcels.....	4-16

4.4.5	Repairs and Maintenance of Nonconformities.....	4-16
4.5	Zoning Subdistrict Development Standards.....	4-17
4.5.1	Limitations and Exceptions to Allowed Uses and Structures.....	4-19
4.6	Standards for Specific Land Uses.....	4-20
4.6.1	Equipment Rental Uses.....	4-20
4.6.2	Live-Work Units.....	4-20
4.6.3	Mixed-Use Development.....	4-22
4.6.4	Outdoor/Sidewalk Dining.....	4-27
4.6.5	Nightclubs and Entertainment Uses- Accessory to Restaurant Uses Only.....	4-28
4.6.6	Parking Structures and Garages.....	4-28
4.7	Off-Street Parking and Loading Standards.....	4-31
4.7.1	Purpose.....	4-31
4.7.2	Regulations for Off-Street Parking.....	4-31
4.8	Landscaping, Walls, and Fences.....	4-33
4.8.1	Landscaping- General.....	4-33
4.8.2	Required Preservation of Native Vegetation and Trees.....	4-33
4.8.3	Landscaping.....	4-34
4.8.4	Walls and Fences.....	4-34
4.9	Signs.....	4-34
4.9.1	Monument Signs.....	4-35
4.9.2	Freestanding Signs in Public Right-of-Way.....	4-35
4.9.3	Pole Signs.....	4-38
4.9.4	Awning Signs.....	4-38
4.9.5	Projecting Signs.....	4-38
4.9.6	Wall Signs.....	4-39
4.9.7	Window Signs.....	4-40
4.9.8	Neon Signs.....	4-40
4.10	Accessory Structures, Equipment, and Utilities.....	4-41
4.11	General Operating Standards.....	4-41
4.11.1	Air Quality.....	4-41
4.11.2	Electrical Interference.....	4-42
4.11.3	Light and Glare.....	4-42
4.11.4	Noise.....	4-42
4.11.5	Vibrations.....	4-42
4.11.6	Outdoor Storage.....	4-42
4.11.7	Parking.....	4-42
4.11.8	Outdoor Activities.....	4-43
4.12	Development Incentives.....	4-43
4.12.1	Lot Consolidation Incentives.....	4-43
4.12.2	Façade Improvement Incentives.....	4-45
4.12.3	Mixed-Use Development Incentives.....	4-46
4.13	Development Review Procedures.....	4-48
4.13.1	Applicability.....	4-48
4.13.2	Uses Allowed by Right.....	4-48
4.13.3	Site Plan Review.....	4-48
4.13.4	Conditional Use Permit.....	4-48
4.14	Highway Environs Overlay District.....	4-49

Table of Contents

4.14.1 Purpose	4-49
4.14.2 Specific Plan Land Use Map Designator	4-49
4.14.3 Review Procedures in the HE Overlay District	4-49
4.14.4 Site Plan Review Procedures- Old Town Specific Plan	4-50
4.14.5 Development Regulations	4-51
4.14.6 Expiration of HE Overlay District and Renewal	4-51
4.14.7 Re-submittal of Development Proposals	4-51

Chapter 5: Design Guidelines

5.1 Introduction.....	5-1
5.1.1 Purpose of the Design Guidelines	5-1
5.1.2 Application of the Design Guidelines	5-1
5.1.3 Using the Design Guidelines	5-2
5.1.4 Organization of Design Guidelines	5-2
5.2 Site Design	5-3
5.2.1 Edge Conditions	5-3
5.2.2 Internal Site Organization	5-13
5.2.3 Relationship to Adjacent Sites and Features	5-14
5.2.4 Parking Lot Design and Configuration	5-15
5.3 Architecture.....	5-16
5.3.1 Architectural Styles	5-16
5.3.2 Building Materials and Color	5-22
5.3.3 Building Form, Mass, and Scale	5-23
5.3.4 Storefront Design	5-24
5.4 Development Details	5-26
5.4.1 Landscaping.....	5-26
5.4.2 Public Spaces and Site Amenities	5-27
5.4.3 Lighting.....	5-28
5.4.4 Building Equipment and Services.....	5-28
5.5 Commercial Signs.....	5-29
5.5.1 Sign Materials and Colors.....	5-29
5.5.2 Encouraged Sign Types	5-30

Chapter 6: Administration & Implementation

6.1 Applicability	6-1
6.2 Amendments to the General Plan	6-1
6.3 Amendments to the Zoning Code and Map.....	6-1
6.4 Administration and Enforcement.....	6-2
6.5 Amendments to the Specific Plan.....	6-2
6.6 Development Review Procedures.....	6-2
6.7 Severability.....	6-2
6.8 Development Strategy	6-3
6.8.1 Development Implementation	6-3

6.9	Financing Strategy	6-6
6.9.1	Financing Implementation	6-6
6.10	Potential Funding Sources	6-7
6.10.1	Funding Sources for Capital Facilities	6-8
6.11	Implementation Matrix	6-16
6.11.1	Timeframes for Implementation	6-17
6.11.2	Implementation Leaders and Teams	6-17
6.11.3	Potential Funding Resources Matrix Key	6-17

Exhibits

Exhibit 1-1:	Vision Plan	1-11
Exhibit 2-1,	Existing Land Use Map	2-3
Exhibit 2-2,	Existing Water Plan	2-7
Exhibit 3-1:	Proposed Land Use Map	3-3
Exhibit 3-2:	Circulation Map.....	3-9
Exhibit 3-3A:	Street Cross-Sections.....	3-13
Exhibit 3-3B:	Street Cross-Sections.....	3-15
Exhibit 3-4:	Proposed Water Plan	3-21
Exhibit 3-5:	Existing and Proposed Drainage Plan.....	3-25
Exhibit 5-1	Edge Conditions.....	5-5

Tables

Table 3-1 –	Land Use Plan Buildout Summary and Change from Existing General Plan.....	3-5
Table 3-2 –	Water Usage Factors (High Desert Water District).....	3-19
Table 3-3 –	Fire-Flow Pipe Dimensions (High Desert Water District).....	3-20
Table 3-4 –	Water Pipeline Replacements Completed (High Desert Water District).....	3-20
Table 3-5 –	Proposed Water Infrastructure Improvements (High Desert Water District).....	3-23
Table 4-1 –	Allowable Uses and Permit Requirements for Districts for Old Town Yucca Valley Specific Plan.....	4-5
Table 4-2 –	General Development Standards and Requirements for Old Town Yucca Valley Districts.....	4-17
Table 4-3 –	Minimum Illumination Levels for Parking Structures and Garages.....	4-29
Table 4-4 –	Lot Consolidation FAR Bonus.....	4-43
Table 4-5 –	Lot Consolidation Residential Density Bonus	4-44
Table 4-6 –	Mixed-Use FAR Incentives.....	4-47
Table 4-7 –	Mixed-Use Residential Density Incentives	4-47
Table 4-8 –	Mixed-Use Development Height Bonus.....	4-47
Table 6-1 –	Financing Techniques and Types of Improvements.....	6-8
Table 6-2 –	Implementation Program & Matrix	6-19

Table of Contents

Appendices

Appendix A - Contact List

Appendix B - Glossary of Terms

Appendix C - General Plan Consistency

Appendix D - Support Information



INTRODUCTION

CHAPTER 1: INTRODUCTION

1.1 Overview

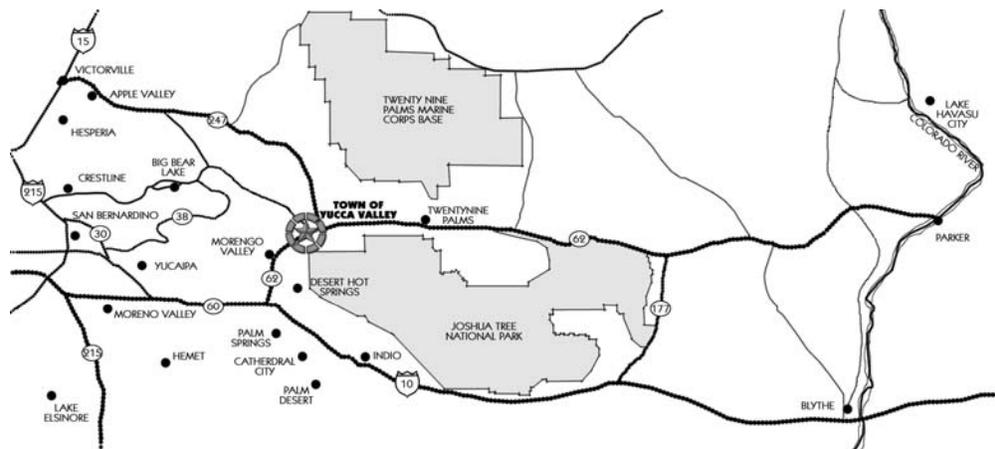
Town. As defined by the American Heritage dictionary, a town is "... larger than a village, but smaller than a city." Indeed, Yucca Valley is a town with a rich past and an even more promising future. According to one long-tenured Yucca Valley resident:

"I live in Yucca Valley for the clean air and to see the stars. I live here because it is a great place to raise children. Yucca Valley is big enough to have modern amenities, but small enough to feel like a small town. It is a community with a rural atmosphere, distinct seasons (unlike the lower desert communities), and a sense of pride that is expressed through a number of annual festivals, such as Grubstakes Days."

The Town of Yucca Valley is a desirable place to live, work, shop, learn, and play. The above message by a resident is an implicit part of the Town's purpose for initiating this Old Town Yucca Valley Specific Plan. Recognizing that growth and change will occur in the community, the Town and the Yucca Valley community have prepared this Specific Plan for Old Town Yucca Valley. This plan will lead and manage the way in which Old Town's built environment eagerly seizes the opportunities of the future, yet pays respect to its community history and values.

1.1.1 Location and Setting

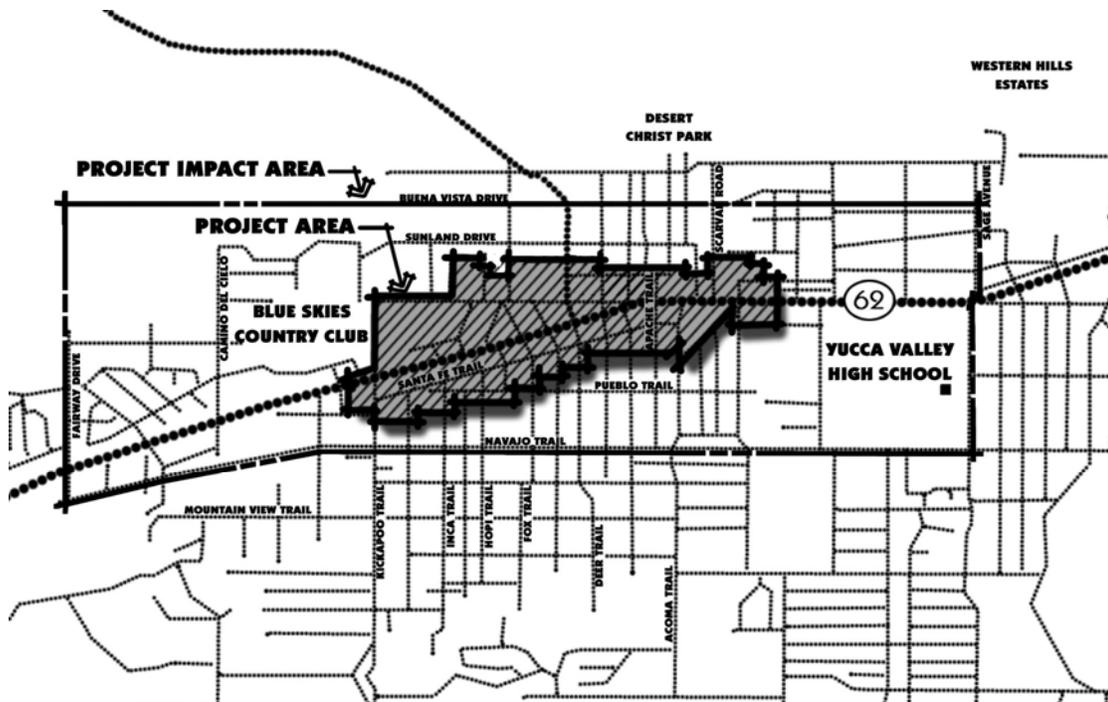
The Old Town Yucca Valley Specific Plan is located near the western end of Yucca Valley along State Route 62 (SR-62).



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Introduction

Old Town Yucca Valley is the original commercial core of the Town of Yucca Valley. The Specific Plan project area includes approximately 250 acres along SR-62 between Church Street on the east and Kickapoo Trail on the west, and between Yucca Trail on the north and just beyond Santa Fe Trail on the south. Today, the major junction in the core of Old Town Yucca Valley is the intersection of Pioneertown Road and the existing alignment of SR-62.



1.1.2 Purpose and Intent

The Town of Yucca Valley has identified the need to improve the economic vitality and livability of the Old Town area by establishing comprehensive strategy to attract and expand economic activity and commerce. The purpose of the Old Town Yucca Valley Specific Plan is to identify key opportunities to enhance the Town’s overall economic base and the historic Old Town area.

The Old Town Yucca Valley Specific Plan serves as a planning and regulatory link between the Town of Yucca Valley General Plan and individual, project-level development within the project area. The Specific Plan provides area-specific land use regulations and development guidelines. Upon adoption by the Town, the Specific Plan provides the legal development requirements for the project area.

The Specific Plan provides a comprehensive set of plans, guidelines, and regulatory standards in addition to administrative and implementation provisions within the land use districts, and applies to residential, commercial, office, mixed-use, and light industrial uses.

The Old Town Yucca Valley Specific Plan has been developed to provide as much flexibility as allowed by State law. The vision of this Specific Plan is intended to be implemented over a 20-year period and, therefore, may be amended over time to reflect the Town’s evolving vision for the area.

This Specific Plan contains flexible concepts, development standards, and design guidelines for the Old Town area that are intended to implement the goals, objectives, and polices of the Town’s General Plan. In developing the Old Town Yucca Specific Plan, the land use plan has been created to achieve the following goals:

- ❖ Implement the General Plan polices by presenting more detailed direction for the Old Town area to improve its overall walkability, traffic circulation, and economic viability.
- ❖ Provide a diversity of housing opportunities that responds to a variety of local needs, incomes, densities, and promote a vibrant Old Town area.
- ❖ Establish high-quality architectural design, in both scale and character, to address the future growth of the area.
- ❖ Develop safe motor vehicle, bicycle, and pedestrian circulation systems, emphasizing the pedestrian experience along the “Main Street” within the Old Town area and mitigating potential future impacts at SR-62 intersections.
- ❖ Carry forward the Vision Plan, guiding principles, and community input and consensus generated during the community outreach program, through more detailed plans, guidelines, and regulations.

1.1.3 Historical Perspective

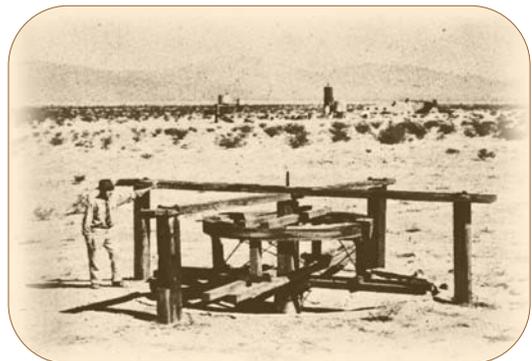
Yucca Valley is part of the Mojave Desert in what is known as the Morongo Basin. The high-desert community is at an elevation of 3,279 feet, 30 miles northwest of Palm Springs, and runs for seven miles along the SR-62 highway north of Joshua Tree to Pipes Canyon and toward other destinations, including the Colorado River. Yucca Valley is noted for its wide-open spaces, healthy lifestyles, equestrian enthusiasts, and clear skies on 320 nights every year, making it a perfect window for celestial observation.

Yucca Valley's low Sonoran Desert exhibits climates ranging from 49 to 100 degrees Fahrenheit, and supports a variety of fauna and flora. The weather includes both dry, howling winds that sweep sand and carve rock, and summer thunderstorms that flood the lowlands relocating sand and rock from higher elevations. Yucca Valley and its surroundings support hummingbirds, scrub jays, desert threshers, roadrunners, desert tortoises, rosy boas, green Mojave rattlesnakes, lizards, mountain sheep, bears, mountain lions, coyotes, and rabbits. Plants like the creosote bush adapted to Yucca Valley's environment over thousands of years, as have the dusty green yucca, saguaro, salt bush, sage, Joshua, and Palo Verde trees with their wispy green foliage.

Natural resources extend well beyond the native fauna and flora to the people who have made Yucca Valley and the Morongo Basin home. From the Native Americans, who migrated from north and south, to miners and prospectors, who traveled from the eastern United States and other countries seeking a better life and wealth, to ranchers, who drove cattle through the Pioneer Pass until after World War II, and shepherders passing through on the way to grazing land. They make up the fabric woven of strength and determination, from diverse backgrounds and interests, who succeeded through hard work, resilience, resourcefulness, and the entrepreneurial spirit.



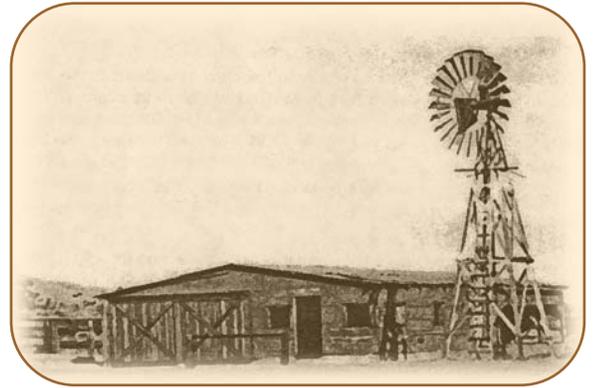
Example of Stamp Mill at Desert Queen Mine in Joshua Tree National Monument
Source: *Yucca Valley Reflections of the Past*



Old Dale Mine
Source: *Yucca Valley Reflections of the Past*

A rediscovery of mineral wealth, known to Native Americans decades before, occurred in the 1860s. After gold was found in Holcomb Valley and near Big Bear, a succession of events brought prospectors and miners, cattle ranchers, and then homesteaders to Yucca Valley. The most successful mines near Yucca Valley were the Onyx, Pipes Canyon, and Rose mines in Pioneer Pass. The Rose Mine, named for its pink and lavender quartz, began operating in the 1880s and was one of the most productive gold mines in the San Bernardino Mountains.

Stamp mills opened in the 1870's and crude housing was built with limited imported timber. In the 1890s, two mining districts (29 Palms District and Monte Negra District) were formed between 29 Palms to Eagle Mountain. Many miners came and went from the 1870s through the 1930s. The high costs associated with transporting water and exporting ore led to the closing of some of the mines surrounding Yucca Valley. Evidence of mining activities faded as the abandoned mine shafts were backfilled. Makeshift cabins with newsprint wallpaper were leveled and Pioneer Pass became a major arterial road to Big Bear.



Warren Well

Source: Morongo Basin Historical Society
(website: www.mbhs.net)

Water has always been as a premium commodity in the dry desert environment. From 1873 to 1881, for instance, cattle ranching had been restricted to the Morongo Valley, where grass and water were accessible and cattle grazed from Whitewater to Big Bear. Around 1880, a teamster named Mark "Chuck" Warren leased government land to raise cattle near Yucca Valley and dug a 160-foot water well. He began selling water, creating the opportunity for other ranchers to move into the area. Over the years, the Warren Well became a gathering place for ranchers during cattle drives and eventually for settlers who staked homesteads between 1910 and the 1930s. Water wells continued to be used until the Yucca Valley Village Company, Ltd. was formed. In 1962, the Hi-Desert Water District was formed and now services the Town of Yucca Valley.



First Yucca Valley Village County
Water District Well

Source: *History of the Morongo Basin*

During its early existence, the San Gorgonio Pass was considered the gateway of the trade route from the Colorado Desert to San Jacinto. Over the centuries, many Indian tribes, Spaniards, fur trappers, and adventurers used the pass. It later served as a stagecoach and rest stop. Supplies were transported from Banning Pass to the Yucca Valley before businesses were established to provide the services

Introduction

needed for the expanding population. Before motor vehicles were available, deliveries came by stagecoach and 8-horse supply wagons, taking two to three days.

Today the major junction in the core of Old Town is the intersection of Pioneertown Road and 29 Palms, which connects Yucca Valley to the historic western movie set in Pioneer Town and continues on SR- 247 toward other high-desert communities, the Twentynine Palms Marine Corps Base, and mountain towns like Big Bear Lake.

The pioneering spirit lingered well into the 20th century. The growth of private enterprise was slow, but sure. In 1923, Lone Star Ranch began to sell gasoline to the public. The first schoolhouse was built in 1927 (possibly in 29 Palms). Ervine Market came into existence in 1936. The Post Office was opened by Hilda Hardesty inside a dry goods store. Yucca Valley Trailer Park opened in 1939.

In 1947, California Water and Telephone Company came to the Morongo Valley, and kerosene continued to be used as fuel for lighting and heat until electricity arrived with Cal Electric in 1948. The first electric utility office opened in Yucca Valley in 1964, when Southern California Edison bought Cal Electric. The first streetlights were installed by Yucca Valley Lighting District in 1959. During the 1970s and 1980s, a housing boom brought rapid population growth.

In November 1991, the Town of Yucca Valley was incorporated. Over the past decade and continuing today, the Town has experienced significant growth and development.

Out of rich historical events sprang a town characterized by mining, ranching, and pioneering influences. As time passed, additional influences came from the proximity of SR-62 and neighboring communities that attracted tourists seeking the sun, natural splendor, remnants of the western movie era, and celestial encounters. Today, as Yucca Valley grows, its leaders and citizens understand the benefits of maintaining the historical character of Old Town while stimulating economic development. In response, the Town of Yucca Valley has facilitated the Old Town Yucca Valley Specific Plan.



Old Town Core Intersection

1.2 The Planning Process

The planning process for the Old Town Yucca Valley Specific Plan included a series of public workshops and meetings to provide opportunities for input by Town residents, business and property owners, developers, and other interested parties. The various workshops and meetings assisted in the development of the overall vision for the Old Town and policy framework for the Specific Plan. The overall visioning exercise provided community consensus and focus to the Old Town Specific Plan area. The planning process for the Old Town Yucca Valley Specific Plan comprised three distinct phases.

1.2.1 Phase I: Information Gathering

Phase I introduced the Urban Design Studio (UDS) team to Old Town Yucca Valley and SR-62. This phase entailed tours with Town staff members, a review of existing documents and information; background and data research; site reconnaissance and visual observations; and a market feasibility analysis. Initial coordination with Caltrans District 8 and other agencies was also completed during this phase.

1.2.2 Phase II: Concepts and Strategies

In Phase II, alternative concepts and strategies were developed. This phase consisted of the creation of an initial strategy contributed to by the community, UDS Team, Town staff, Caltrans, and other stakeholders through hands-on meetings. A four-day community workshop was held in June of 2004 to interview the local stakeholders, tour Old Town with residents and business and property owners, and a hands-on design workshop for interested participants. This workshop and subsequent charrette led to the creation of the initial visions, concepts, frameworks, plans, and strategies for Old Town and SR-62. This initial vision was presented to community stakeholders, including the Old Town merchants and property owners, for feedback to shape the draft Specific Plan.

As part of Phase II, meetings with Caltrans District 8 were held to consider the possibility of rerouting the SR-62 highway around the Town and creating opportunities for a more vibrant, walkable Old Town. At the time of preparation of this document, Caltrans District 8 had prepared, and recommended approval, of a Project Initiation Proposal that identifies a total of four SR-62 realignment alternatives. Through discussions with Caltrans, City staff, and stakeholders, a preferred Alternative D was identified. The recommended SR-62 Realignment Alternative D would realign SR-62 from Kickapoo Trail north, transitioning to Yucca Trail in the vicinity of Apache Trail. As part of the Realignment Alternative D, SR-62 and Yucca Trail to the north would become a six-lane divided highway, with a single lane connecting to the existing alignment of SR-62 through Old Town (Main Street – Old SR-62). The single-lane roadway would transition to a two-lane divided industrial roadway allowing Old Town Yucca Valley “Main Street” to become more pedestrian-oriented. The SR-62 Realignment concept plans provided by Caltrans District 8 are

purely conceptual, and are utilized only for visioning purposes. The final design and ultimate right-of-way of an SR-62 realignment will be determined at a later date.

1.2.3 Phase III: Preparation of Specific Plan

Phase III comprised drafting and completing the Specific Plan, during which meetings and public hearings with Caltrans and the Town were held to review and discuss progress on the Specific Plan and SR-62 realignment alternatives. The vision poster was introduced to the community as a visual illustration of the community's hopes and expectations for Old Town.

1.3 Community Outreach

The Vision and Specific Plan are based on a comprehensive community involvement process that included the following events:

- ◆ February 19, 2004 – Town Council initiates process
- ◆ March 30, 2004 – Kickoff meeting with Town staff, Caltrans, and other partners
- ◆ June 2-11, 2004 – Community Immersion Program:
 - Community Workshop 1: Understanding Old Town and SR 62
 - Community Leaders Kickoff Breakfast
 - One-on-One Interviews
 - Storefront Drop-In Interviews
 - Pancake Breakfast & Old Town Walk
 - Old Town Yucca Valley Issues, Ideas, Values, and Visions Workshop
 - Old Town Hands-On Design Workshop and Presentations
 - Off-Site Design Studio
 - Town Staff Meeting
 - Community Leaders Reception
 - Meeting with Old Town Merchants and Property Owners
 - Caltrans Workshop
 - Community Open House
 - SR-62 Mobile Design Workshop
- ◆ July and August, 2004 – Meetings with Caltrans and Town
- ◆ September 2, 2004 – Planning Commission and Town Council Workshop to:
 - Update Town Council and Planning Commission on Old Town Specific Plan and SR-62 Streetscape Master Plan
 - Review and Discuss Old Town SR-62 Alternatives
 - Review and Discuss General Plan “Highway Roadway” Cross-Section
 - Receive Town Council and Planning Commission Input and Guidance
- ◆ November 18, 2004 – Planning Commission and Town Council Workshop, including:
 - Planning Process Update: Where We Are
 - General Plan Amendment “Highway Roadway” Cross-Section
 - Old Town SR-62 Alternative
 - Overview of Old Town Market Feasibility Study, General Economic Benefits of Addressing SR-62, and Possible Funding
 - Partnerships & Where We Go from Here
- ◆ December 10, 2004 – Meeting with Caltrans at Town to introduce partnership
- ◆ January 24, 2005 – Meeting with Caltrans at Town to review project

- ◆ March 24, 2005 – Project Initiation Proposal prepared by Caltrans
- ◆ May 19, 2005 – Project Initiation Proposal approved by Caltrans
- ◆ May 25 and 26, 2005 – Confirm New Directions: Old Town Yucca Valley
- ◆ September 18, 2007 – Joint Workshop with Traffic Commission, Planning Commission, and Town Council
- ◆ September 18, 2008 – Planning Commission Hearing
- ◆ November 27, 2007 – Community Meeting to review Public Review Draft Specific Plan
- ◆ November 8 and December 13, 2007 – Town Council Hearings

1.4 Vision Plan

“Imagine . . .

Imagine . . . Old Town Yucca Valley as a vibrant destination in the Morongo Basin where history, environment, art and culture, education, shopping, living, and working coincide amidst a very walkable downtown.

Imagine . . . Old Town Yucca Valley as a renewed setting with its own “Main Street,” mix of housing, diversity of shopping and working environments, and inviting public spaces.

Imagine . . . Old Town Yucca Valley as a district that is highlighted with eclectic architecture, an abundance of indigenous landscaping, comfortable pedestrian areas, and public art.

Imagine . . . Old Town Yucca Valley as a gathering place for respite and discovery by community members and visitors alike.

Imagine . . .”

This vision was derived through an intensive public involvement process consisting of ten days of workshops with Town staff, the community, and Caltrans District 8. The accumulated recommendations, ideas, and efforts of all who attended the immersion week were then synthesized into four alternative concept plans for the Specific Plan area, ultimately refining those alternatives to the proposed Vision Plan (Exhibit 1-1). The following summary of the Vision Plan highlights the improvements needed to implement this vision to create a vibrant Old Town Yucca Valley “Main Street” environment.

The Vision Plan draws inspiration from Old Town’s storefront character reminiscent of past development and incorporates common local features of the desert environment. To implement the vision, unique land use districts were developed to separate and distinguish the historic Old Town from other developments outside of the core area (“Main Street”). The character of the Old Town core is planned to be more urban in the density of the buildings. Streetscape and landscaping elements are arranged, while current development patterns in outlying area are less dense and suburban in character. To also identify the Old Town Districts, gateway signage directs tourists into Old Town, while directional signs guide visitors throughout the town. Accompanying the improved signage are enhanced landscape and streetscape features that add to the comfort of both residents and tourists.

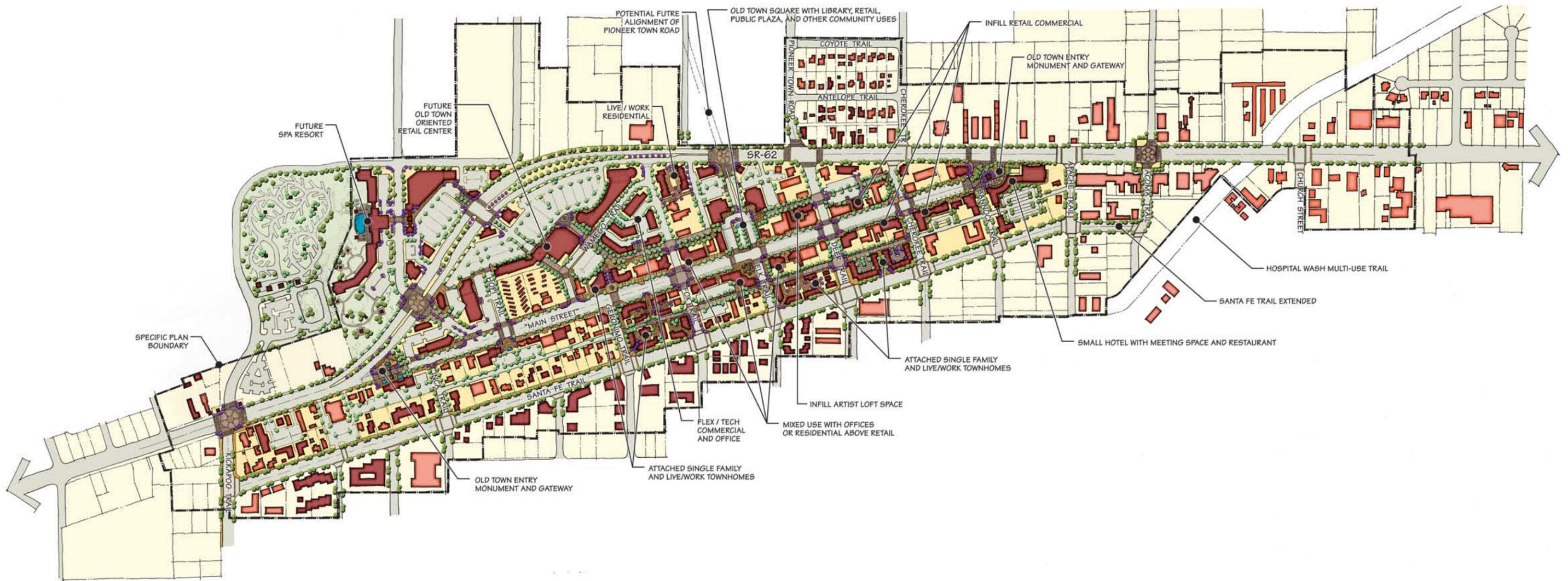
Introduction

The future landscaping utilizes indigenous plants and materials, which are prevalent in the high desert, and the design concepts communicate a more natural appearance and maintain the character of this area. The presence of plants in otherwise barren terrain creates an inviting and shaded place for the residents to rest on benches when pausing between the shops clustered along major streets or when waiting for the public transportation. It is also an enticement to tourists venturing through Town looking for amenities to satisfy their basic needs. The visual character is extended along the corridors of major intersections, including Fox Trail, Pioneertown Road, and Bannock Trail – new hubs in the Old Town district core. Enhanced landscaping along these streets draws attention to and prompts further investigation of restaurants, entertainment, and shopping opportunities at unique stores on the cross streets.

To ensure the continued economic development on both sides of Main Street/SR-62, pedestrian accessibility to both sides of the street must be safe and convenient. Solving the problem of wide street crossings for pedestrians was a serious consideration and has been addressed by creating “bump outs” at intersections, signaling the major intersections, and improving striping at crosswalks. As a result, pedestrian crossing time is shorter and merchants have equal opportunities for increased business. The wide streets accommodate more parking spaces, while curbs and gutters have been introduced to formally designate parking areas.

The streetscape improvements include defining the perimeter of the Old Town district core area as a designated bicycle path that runs along two major streets (Yucca Trail Road and Santa Fe Road) to the north and south of SR-62. The bicycle path improves connectivity, accessibility, and safety for residents traveling to and from the commercial core and public services. The Old Town district core area is graced with a public library, civic center, and natural history museum that share a common open space where community youth can gather. Seasonal programs are scheduled to educate and entertain residents and tourists.

Old Town plays an important role in the overall physical, economic, and social setting of Yucca Valley, and has numerous opportunities for synergy with adjacent and nearby community assets. The Blue Skies Country Club is one such amenity that has untapped potential to be enhanced and to more effectively serve as a source of economic development opportunities within Old Town and adjacent areas. Likewise, the Hi-Desert Water District Property, spanning Pioneertown Road just north of Old Town, has a range of envisioned opportunities that could benefit from and contribute to Old town’s vitality. These two amenities are among others within the context of Old Town that could be harnessed to uplift vitality, reinforce identity, and honor heritage to the benefit of the Yucca Valley community.



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1.5 Organization of the Specific Plan

The Old Town Yucca Valley Specific Plan was derived out of an intensive planning process consisting of numerous workshops over 16 months. The Specific Plan creates five new planning districts, includes design guidelines, and presents streetscape concepts to foster a mixed-use environment that is pedestrian friendly and has been developed in a manner largely consistent with traditional small-town downtown core.

The Old Town Yucca Valley Specific Plan is organized into six chapters as follows:

- ◆ Chapter 1: Introduction to the Old Town Specific Plan – This chapter presents the purpose and intent, a historical context, planning process, summary of the Specific Plan, Specific Plan authority and requirements, statement of CEQA compliance, and other pertinent information.
- ◆ Chapter 2: Existing Conditions - This chapter discusses the existing on and off-site land uses and infrastructure present at the time of preparation of this Specific Plan.
- ◆ Chapter 3: Development Plan for Old Town – This chapter describes the land use concept, urban design, wayfinding signage, public space and streetscape plan, circulation and public parking concept, and infrastructure plan.
- ◆ Chapter 4: Development Regulations for Old Town – This chapter defines land use districts, the permitted land uses, site development standards, parking requirements, sign regulations (as appropriate beyond existing Town standards), landscaping requirements, and development incentives for Old Town.
- ◆ Chapter 5: Design Guidelines – This chapter serves as a toolkit of suggestions and ideas that illustrate Old Town design concepts through text, graphics, and photographs for a variety of land uses, building types, edge conditions, signage types, landscaping, and site planning.
- ◆ Chapter 6: Administration and Implementation – This chapter specifies the procedures for amending, administering, and enforcing the Specific Plan, development review procedures, mitigation monitoring, public improvement costs (to an order of magnitude), and financing mechanisms.
- ◆ Appendices – This chapter provides background material related to the Specific Plan update process, including a list of contacts and persons consulted, a glossary of Specific Plan terminology, General Plan consistency discussion, and related relevant support information.

1.6 Specific Plan Authority and Scope

1.6.1 Authority

The Old Town Yucca Valley Specific Plan is established through the authority granted to the Town of Yucca Valley by California Government Code, Title 7, Division 1, Chapter 3, Article 8, Sections 65450 through 65457 (Specific Plans).

The Specific Plan is regulatory in nature, and serves as the zoning law for the properties within the boundaries of the Specific Plan. Development plans, site plans, and tentative tract and parcel maps must be consistent with the Specific Plan and General Plan. The scope of the subjects covered in the Specific Plan is the same as that of the General Plan to the extent that the subject under consideration involves the Old Town Yucca Valley portion of the City. As expressed in California law, Specific Plans may be adopted either by ordinance or by resolution. This allows jurisdictions to choose whether their specific plans will be policy driven (adopted by resolution) or regulatory in nature (adopted by ordinance).

1.6.2 Relationship to the General Plan

The current Town of Yucca Valley General Plan was last updated in 1995. The Specific Plan reflects current land use buildout and other development policies, as described within the General Plan, while also addressing the realignment of SR-62 and providing opportunities for a vibrant Old Town mixed-use district through increasing density and intensifying uses. The General Plan Amendment accompanying the Old Town Yucca Valley Specific Plan amends the land uses, intensities, and circulation within the project area.

1.6.3 Relationship to the Zoning Ordinance

The Old Town Yucca Valley Specific Plan provides customized, detailed standards and guidelines for future development within the project area. Unless otherwise stated, the Specific Plan development procedures, regulations, and standards shall supersede the relevant provisions of the Town's Zoning Ordinance as they currently exist, or as are subsequently amended in the future. Any development regulation not directly addressed in this Specific Plan shall be subject to the City's adopted zoning regulations in place at the time of the individual request.

Early in the design process, it was determined that the existing Zoning Ordinance had not anticipated the development density and intensity necessary to create a vibrant mixed-use environment within the Old Town district. As a result, new guidelines, standards, and regulations are necessary to implement the Community Vision Plan.

Successive approvals such as, tentative tract maps, parcel maps and other development approvals must be consistent with both the Old Town Yucca Valley Specific Plan and the Town of Yucca Valley Zoning Ordinance.

1.6.4 Compliance with the California Environmental Quality Act (CEQA)

Concurrent with the preparation of the Old Town Yucca Valley Specific Plan document, the Program Environmental Impact Report (EIR) will be prepared in conformance with the California Environmental Quality Act (CEQA) (California Public Resources Code Section 21000 [PRC] [18] et seq.); California CEQA Guidelines (California Code of Regulations, [CCR] Title 14, §15000 et seq.), and the rules, regulations, and procedures for implementation of CEQA; as adopted by the Town. The principal CEQA Guidelines sections governing content of this document are §§15120 through 15132 (Content of an EIR), §15168 (Program EIR), and §15161 (Project EIR).

In accordance with §15121 of CEQA as noted above, the primary purpose of the Program EIR is to provide decision-makers and the public with specific information regarding the environmental effects associated with the Project; identify ways to minimize the significant effects; and describe and analyze reasonable alternatives to the Project. Mitigation measures will be provided that may be adopted as Conditions of Approval to reduce the significance of impacts resulting from the Project. The Program EIR will analyze the environmental effects of the Project to the degree of specificity appropriate to the current proposed actions, as required by §15146 of the CEQA Guidelines. This Program EIR will discuss both the direct and indirect impacts of the Project, as well as the cumulative impacts of other past, present, and reasonably foreseeable future projects. CEQA requires the preparation of an objective, full-disclosure document to inform agency decision-makers and the general public of the direct and indirect environmental effects of the proposed action; provide mitigation measures to reduce or eliminate significant adverse effects; and to identify and evaluate reasonable alternatives to the proposed Project.

This Program EIR will also serve as the basis for future tiered environmental analyses for projects resulting from implementation of The Old Town Yucca Valley Specific Plan. The future analyses will be developed as more project-specific information is defined and more detailed architectural and engineering plans are prepared. Subsequent activities in the Project must be examined in light of the Program EIR to determine whether additional environmental documents must be prepared.

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Chapter

2

EXISTING CONDITIONS

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CHAPTER 2: EXISTING CONDITIONS

2.1 Existing Land Uses

2.1.1 On-Site Land Uses

Existing land uses within the Old Town Yucca Valley Specific Plan area are residential, commercial, industrial, and civic. Commercial land uses, which include retail, medical, restaurants, offices, and hotels, are primarily located along SR-62; residential and service commercial uses bound on the north and south, and vacant land is located throughout. The northwest corner of the area contains mostly industrial uses of varying degrees, including automobile repair shops and warehouses. Refer to Exhibit 2-1, *Existing Land Use Map*.

Various easements are located within the project site (including storm drainage facilities and utility easements) that overlay other land uses (commercial, residential, and roads).

San Bernardino County Flood Control District. The San Bernardino Flood Control District drainage facilities consist of the Yucca Wash, Church Channel, and Water Canyon Channel.

The Yucca Wash runs northeast to southwest, crossing SR-62 near Church Street, and ending at Deer Trail. Portions consist of 100' to 150' San Bernardino County Flood Control District FEE, a 100' easement and a 30' right-of-way.

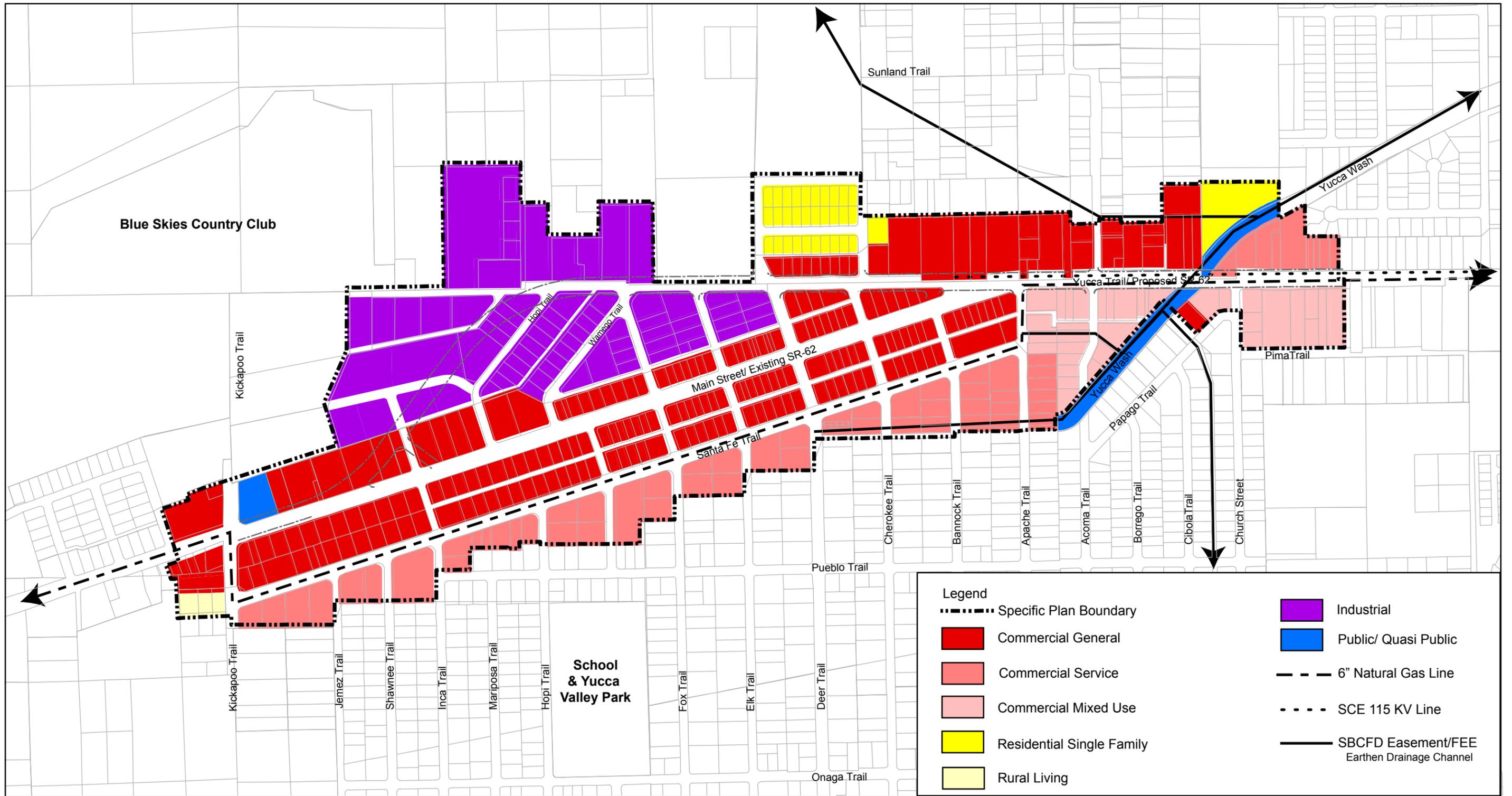
Church Channel runs south from Yucca Wash parallel to Church Street to Onaga Trail, and consists of a 30' right-of-way.

Water Canyon Channel runs northwest to southeast from Water Canyon Basin and merges with Yucca Wash between Church Street and Palm Avenue. It consists of a 100' San Bernardino County Flood Control District FEE.

Southern California Gas Company. A natural gas line runs through the existing street right-of-way through the project area along Santa Fe Trail to Apache Trail, then north to SR-62/Twenty Nine Palms Highway, and continuing to the east.

Southern California Edison. An underground electrical line exists along the street right-of-way on SR-62/Twenty Nine Palms Highway, starting at the junction of Yucca Trail and SR-62/Twenty Nine Palms Highway, and continuing to the east.

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2.1.2 Surrounding Land Uses

Immediately surrounding the Old Town Yucca Valley Specific Plan area is a mixture of single-family residential, commercial mixed-use, service uses, and vacant land.

2.2 Existing Improvements

Within the Specific Plan area, improvements to the drainage and wastewater facilities are planned. These proposed improvements are noted in the Yucca Valley Master Plan of Drainage 1998 and the Hi-Desert Water District Wastewater Treatment Plant Technical Specifications 2001.

2.3 Topography

The Old Town Yucca Valley Specific Plan area is located in a valley that gently slopes south, and is from 3,320 to 3,340 feet above sea level at its lowest point. The area has a low susceptibility to rockslides and landslides.

2.4 Circulation and Access

2.4.1 Vehicular

The Old Town Yucca Valley Specific Plan area is located along SR-62, the regional access road, which runs east-west through Yucca Valley. The Town's General Plan designates it as a six-lane divided highway. Pioneertown Road, a designated four-lane collector road, serves both the regional and local population with access to Yucca Valley from the north.

The secondary roads running east-west in the Old Town Specific Plan area are Yucca Trail, a two-lane industrial road, and Onaga Trail, a four-lane divided arterial. Santa Fe Trail is a designated four-lane collector road, and runs northeast-southwest. Kickapoo Trail, Deer Trail, and Acoma Trail are secondary roads running north-south, and are designated as four-lane collectors.

The Morongo Basin Transit Authority (MBTA) provides transit service throughout Yucca Valley, including within the Specific Plan area.

2.4.2 Pedestrian

Pedestrian trails are proposed within the Old Town Specific Plan area, as specified in the Parks Master Plan (1999) and Draft Trails Master Plan (2002). The existing trails include multi-use trails and bicycle trails along existing roadways, washes, and drainage channels.

Existing Conditions

2.5 Infrastructure and Utilities

2.5.1 Water

The Town of Yucca Valley lies within the Warren Valley Hydrologic Subarea. The Town's sole source of water is the Warren Valley Groundwater Basin, which is recharged by the Morongo Basin Pipeline. The Hi-Desert Water District (HDWD) serves the Old Town Yucca Valley Specific Plan area. Today, the District has approximately 8,400 service connections and a service area that spans approximately 52 square miles. In addition, the District operates 16 storage tanks and 17 wells, and maintains nearly 300 miles of transmission and distribution pipelines. Refer to 2-2, *Existing Water Plan*.

2.5.2 Wastewater

Currently, all wastewater is treated through septic systems located on each lot. Because of concentrations of nitrates at some wells and the decline of the water aquifer, the HDWD is continuing to explore the feasibility of constructing a wastewater collection and treatment facility to serve Yucca Valley. The Town of Yucca Valley Public Works Department is to update as information becomes available.

2.5.3 Storm Drainage

In 1999, the Town of Yucca Valley Master Plan of Drainage was completed to provide for the future development of adequate drainage facilities. At the time, there were limited drainage facilities to mitigate flooding. Within the Specific Plan area, the main existing stormwater collector is Yucca Wash, an earthen trapezoidal flood control channel. Currently, a large portion of the Specific Plan area is located within the 100-year flood plain. The Town of Yucca Valley Public Works Department is to update as information becomes available.

2.5.4 Electricity

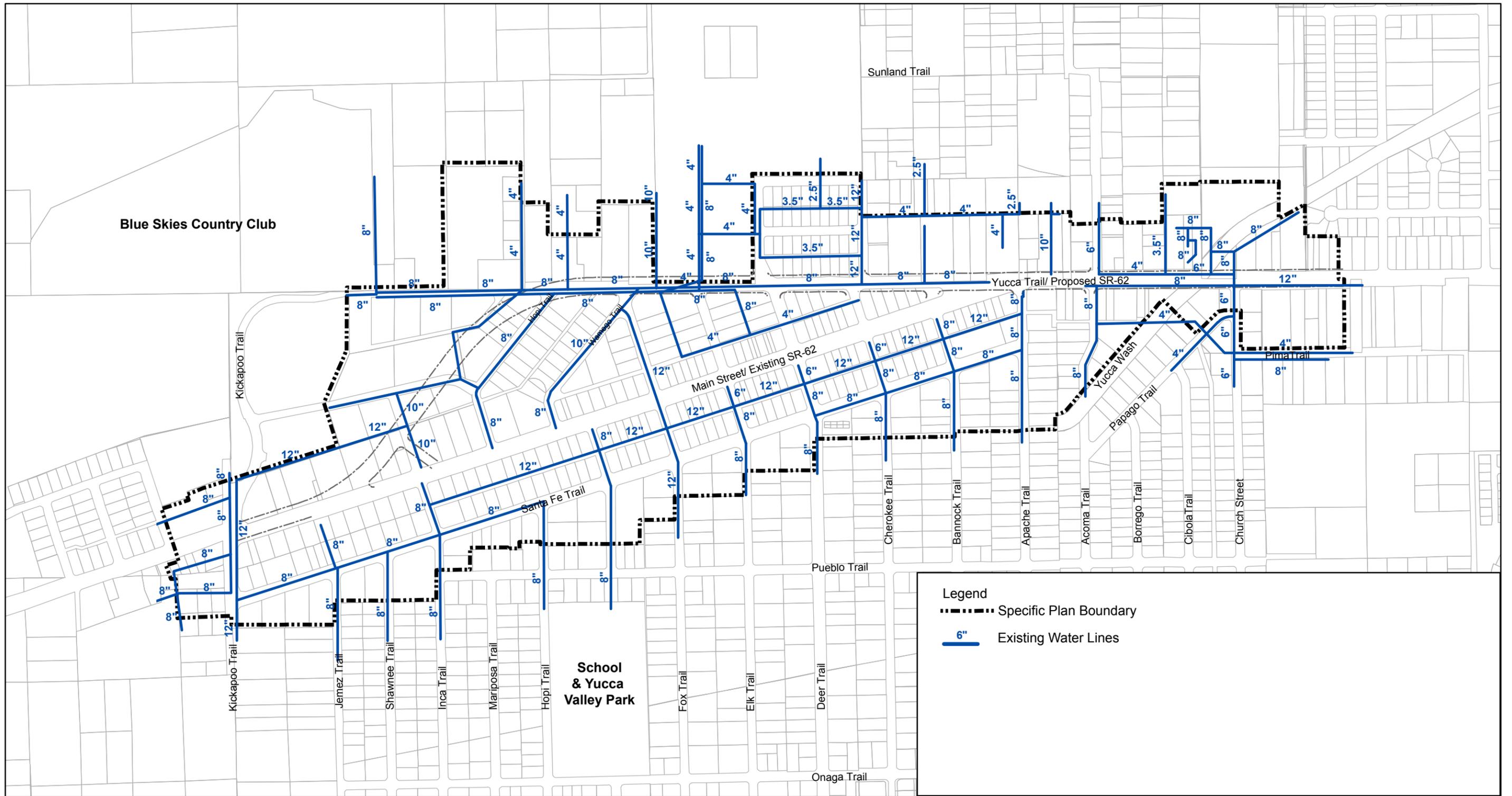
Southern California Edison (SCE) provides electricity in the Old Town Yucca Valley Specific Plan area. The primary transmission line is located along SR-62, converging with the Devers Substation near Desert Hot Springs to the west and with the Hi-Desert Substation in Twentynine Palms to the east.

2.5.5 Natural Gas

The Southern California Gas Company provides natural gas service to the Old Town Yucca Valley Specific Plan area. Two-, four-, and six-inch transmission and distribution lines run throughout Yucca Valley. Currently, there is a six-inch gas line running under SR-62.

2.5.6 Communication Systems

GTE California provides business and residential telephone service to Yucca Valley and other Morongo Basin communities. Century Communication provides cable television.



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2.5.7 Solid Waste

Hi-Desert Disposal provides solid waste disposal services for both commercial and residential customers. The collected trash is disposed of at the Landers and Morongo Valley Landfills. Recycling is provided by Hi-Desert Recycling.

2.6 Seismic

The Town of Yucca Valley is located within a fault-controlled basin that extends westward through Morongo Valley and eastward beyond Twentynine Palms. The Town lies within intensity zone VIII, as defined by the Modified Mercalli Intensity Scale. Four major faults are within the Town of Yucca Valley, one of which, the Pinto Mountain Fault, is located within the Old Town Yucca Valley Specific Plan area. The Pinto Mountain Fault is a major east-west fault extending through the center of Yucca Valley from the base of the Sawtooth Mountains eastward along SR-62. The fault is estimated to be capable of producing a maximum earthquake of magnitude 7.4.

2.7 Environmental Resources

2.7.1 Biological Resources

There are many biological resources within the Town of Yucca Valley. The area of the Old Town Yucca Valley Specific Plan area is low in biological resources, but does border high-value biological resources to the north. Throughout the Town, Joshua trees and the desert tortoise are considered rare species. In or near the Specific Plan area, the vermilion flycatcher, summer tanager, and yellow-billed cuckoo birds are sensitive species.

2.7.2 Cultural Resources

The Town has many cultural resources of archeological or historical importance, and has many resources that need to be surveyed. Currently, the Old Yucca Elementary School, known as the "Scout House," has been recognized by the town as having local historic significance and is located within the Old Town Yucca Valley Specific Plan area.

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Chapter

3

DEVELOPMENT PLAN

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CHAPTER 3: DEVELOPMENT PLAN

This chapter summarizes the vision, the preferred land use plan, the streetscape/community design, and the associated infrastructure improvements necessary to accommodate the future build-out of the Old Town Yucca Valley Specific Plan.

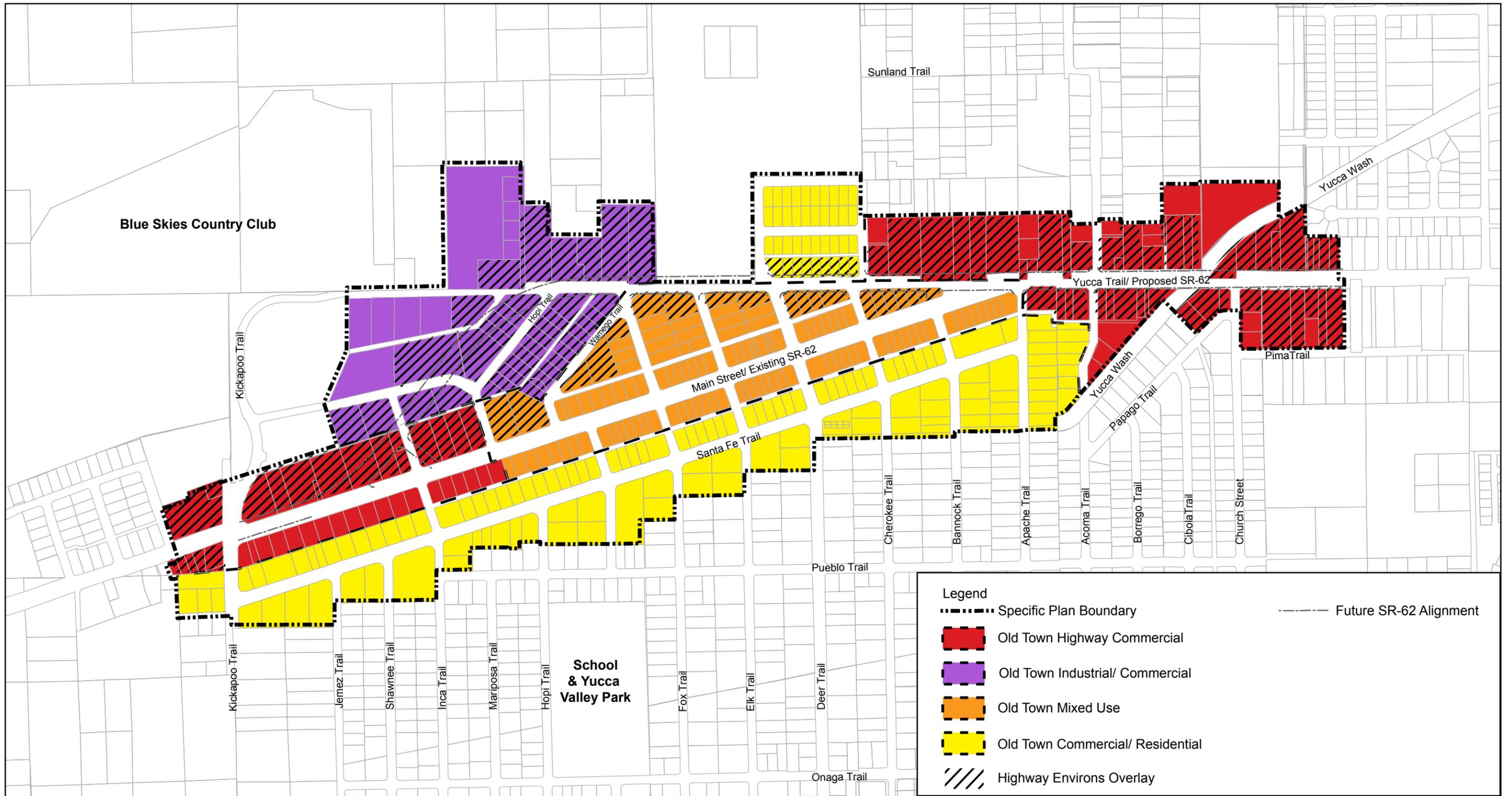
3.1 Land Use

The land use plan for the Old Town Yucca Valley Specific Plan provides for the development of four planning districts: the Old Town Mixed-use District, Old Town Commercial/Residential District, Old Town Industrial/Commercial District, and the Old Town Highway Commercial District. An additional overlay district, the Highway Environs Overlay, provides additional development requirements for those areas that may be affected by the potential realignment of SR-62 and require additional discretionary review. Exhibit 3-1, *Proposed Land Use Map*, depicts the boundaries of the planning districts and potential realignment of SR-62 within the project area; Table 3-1, *Land Use Plan Buildout Summary*, provides a statistical breakdown of each district.

As shown in Table 3-1, the Specific Plan will allow a maximum of 1,116 residential units and up to 2,900,604 square feet for a variety of uses, from commercial/retail and office uses, to civic. The net change between the Specific Plan and the existing *Town of Yucca Valley General Plan* would be an increase of 1,089 dwelling units and a reduction in commercial/retail/industrial uses of 478,435 square feet.

Following is a description of the planning districts.

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Legend

- Specific Plan Boundary
- Future SR-62 Alignment
- Old Town Highway Commercial
- Old Town Industrial/ Commercial
- Old Town Mixed Use
- Old Town Commercial/ Residential
- Highway Environs Overlay

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Table 3-1
Land Use Plan Buildout Summary and Change from Existing General Plan

District and Land Use Type(s)	Specific Plan Buildout		Specific Plan Buildout Net Change from General Plan	
	Dwelling Units	Square Feet	Dwelling Units	Square Feet
OLD TOWN MIXED-USE				
Commercial/Retail – up to 1.00 FAR Residential – up to 40 du/ac	465	759,317	465	550,505
OLD TOWN HIGHWAY COMMERCIAL				
Commercial/Retail – up to 0.35 FAR Residential – none	0	889,684	(16)	(304,760)
OLD TOWN COMMERCIAL/ RESIDENTIAL				
Commercial/Retail – up to 0.40 FAR Residential – up to 24 du/ac	413	699,769	402	(413,773)
OLD TOWN INDUSTRIAL/ COMMERCIAL				
Industrial/Commercial – up to 0.40 FAR Residential – up to 30 du/ac	238	551,834	238	(310,407)
TOTALS	1,116	2,900,604	1,089	(478,435)

FAR = floor-to-area ratio; du = dwelling unit(s); ac = acre(s)

Highway Environs Overlay District identifies only those areas that require additional discretionary review by the Town of Yucca Valley relative to the realignment of SR-62.

3.1.1 Old Town Mixed-Use

The Old Town Mixed-use District is the core downtown district of the Specific Plan, with the potential for development of 759,317 square feet of commercial retail uses (at a maximum floor-to-area ratio [FAR] of 1.00) and 465 residential units (at a maximum of 40 dwelling units [du] per acre [ac]).

General features include:

- ◆ Establishes Core Downtown Area
- ◆ Establishes compact, vertical mixed-use development
- ◆ Mixes complimentary uses: higher-intensity residential, commercial and smaller-scale retail development
- ◆ Expands housing opportunities
- ◆ Provides street-oriented, pedestrian-oriented development
- ◆ Enhances streetscape

3.1.2 Old Town Highway Commercial

The Highway Commercial District serves as the primary eastern and western gateways to the Old Town Specific Plan and provides SR-62-oriented commercial development with the potential for 889,684 square feet of commercial and retail uses (FAR of 0.35).

General features include:

- ◆ Enhances streetscape
- ◆ Caters to the local and regional markets
- ◆ Provides a wide range of retail sales, business, and personal services
- ◆ Orients primarily to the automobile customer

3.1.3 Old Town Commercial/Residential

The Downtown Commercial/Residential District provides a complementary mix of commercial and residential development with the potential for development of 699,769 square feet of commercial retail uses (maximum FAR of 0.40) and 413 residential units (maximum of 24 du/ac).

General features include:

- ◆ Buffers Old Town Mixed-use District from residential areas
- ◆ Offers development that respects adjacent residential development
- ◆ Provides street-oriented, pedestrian-oriented development
- ◆ Mixes commercial, office and residential uses
- ◆ Enhances streetscape

3.1.4 Old Town Industrial/Commercial

The State Route 62 (SR-62) Industrial District provides a variety of industrial/commercial and residential uses near SR-62 with the potential for development of 551,834 square feet of industrial/commercial uses (maximum FAR of 0.40) and 238 residential units (maximum of 30 du/ac).

General features include:

- ◆ Mixes light industrial, flex-tech, small-scale manufacturing, service commercial, and limited live-work residential development uses
- ◆ Serves as a primary node for local commercial activity
- ◆ Enhances streetscape

3.1.5 Highway Environs Overlay

The Highway Environs Overlay District provides a heightened level of discretionary review for development proposals in areas where land use regulations may be changed, depending on the future realignment of SR-62. This district ensures that future development proposals are not adversely affected by the potential future realignment of SR-62.

3.2 Community Facilities

The Old Town Yucca Valley Specific Plan provides the Town an opportunity to develop a variety of public facilities to enhance the Old Town area and provide a centralized community meeting place. Some of the public facilities envisioned within the general vicinity of the “Main Street” are a new town hall and library, a cultural center, a museum, governmental service facilities, and a public square.

3.3 Circulation Plan

The Specific Plan’s *Circulation Plan*, Exhibit 3.2, includes a semi-grid system of roadways, emphasizing community and regional linkages to the Old Town area and addressing the potential realignment of SR-62. A “Main Street” is proposed (within the existing SR-62 alignment) that extends through the center of the Old Town. The “Main Street” design incorporates an enhanced gateway from SR-62 and traffic-calming measures to reduce traffic speeds, enhance pedestrian safety, and promote walkability of the area. In addition, many of the alleys adjacent to the “Main Street” are designed with pedestrian shoulders to be more pedestrian-friendly, and to enhance alternative connections. The Circulation Plan also identifies the potential SR-62 realignment location and conceptual Gateway lane configurations, currently being studied by Caltrans District 8.

As a component of this project, a comprehensive traffic assessment was conducted to identify existing conditions and to forecast future conditions, based upon the proposed SR-62 Realignment. Refer to Exhibit 3-2, *Circulation Map*, and Exhibits 3-3A and 3-3B, *Street Cross-Sections*.

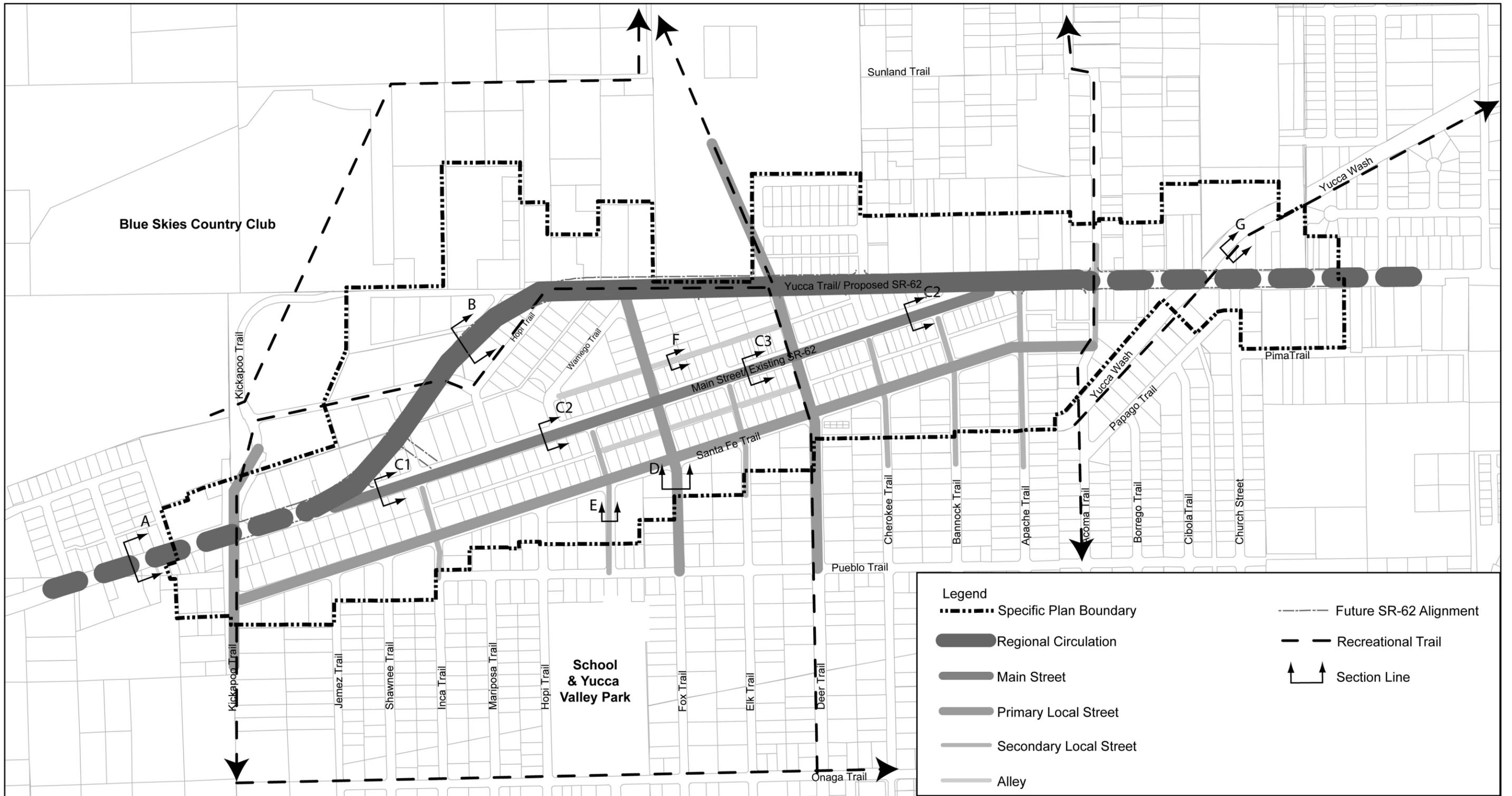
3.3.1 Linkages

Linkages (both local and regional) to the Old Town area are critical to its overall vibrancy. Developing a logical system of linkages and connections to and between the Old Town area and other areas of the Town of Yucca Valley for bicyclists, pedestrians, and automobiles will contribute to the Old Town revitalization; at the core of this issue are the potential SR-62 Realignment and the local street system. The Circulation Plan articulates the envisioned network of roadways to optimize connections, identify gateways, and develop alternatives for moving around Old Town. With the SR-62 Realignment, a more pedestrian-friendly “Main Street” environment would evolve through the center of the Old Town, enhancing community pedestrian and bicycle linkages. By working toward an interconnected system of well-designed, attractive streets that balances the needs of pedestrians, bicyclists, and automobiles, the vitality of Old Town will ultimately be enhanced.

3.3.2 Roadway Network

As described in this subsection, the roadway network includes a variety of cross-sections to encourage a more pedestrian-friendly environment.

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Proposed SR-62/Yucca Trail

Proposed SR-62/Yucca Trail is identified in the General Plan as a 6-lane divided highway. The SR-62 ultimate right-of-way (ROW) is 134 feet, including a 12-foot median with three travel lanes each DIRECTION, an 8-foot bike/emergency lane adjacent to the curb, and 5-foot sidewalks. The portion of SR-62 west of Kickapoo Trail and a portion east of Apache Trail, within the Specific Plan area, will be designed with 5-foot sidewalks, while the proposed SR-62 Realignment portion of SR-62 will have a wider sidewalk. The specific SR-62/Yucca Trail alignment and design alternatives will be determined through future studies by Caltrans District 8 in cooperation with the Town of Yucca Valley.

Gateway access points are proposed for access between SR-62 and the “Main Street,” and are conceptually depicted below. At the Western Gateway, an eastbound, one-way travel lane will connect to the proposed “Main Street” west of Inca Trail, while a westbound lane from the “Main Street” to SR-62 will be located north of Inca Trail. Refer to Cross-Sections A and B. The Eastern Gateway will include the extension of “Main Street” from Bannock Trail east to SR-62, with one inbound lane and two outbound lanes.

Additional discussion of the proposed SR-62 Realignment is in Section 3.3.3.



Western Gateway



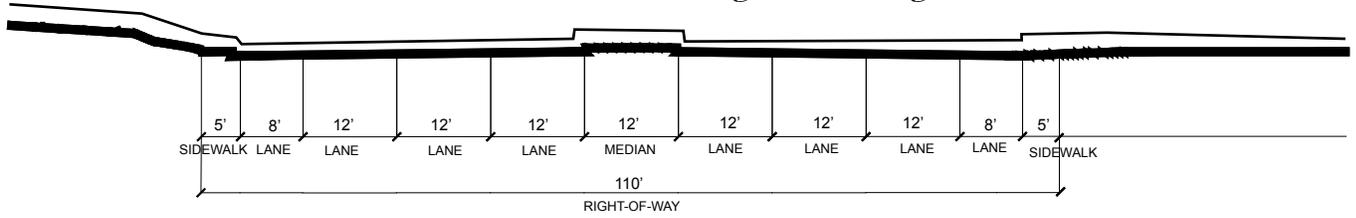
Eastern Gateway

“Main Street” (110’ ROW)

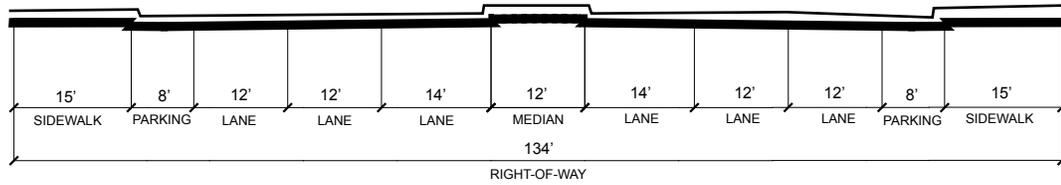
Main Street is designed to promote pedestrian activity by narrowing the ROW and providing on-street storefront parking. The Main Street is proposed as a 110-foot ROW within the existing roadway alignment from the Western Gateway to Bannock Trail, where it is proposed to be realigned to transition north to SR-62, creating the Eastern Gateway. Main Street at the Eastern Gateway cross-section includes a one-way travel lane, 8-foot parking lanes, and a 35-foot sidewalk and public plaza area on both sides of the road (Section C-1). In the vicinity of Inca Trail, Main Street transitions to two-way travel lanes separated by a 12-foot left-turn lane, with 8-foot parallel parking areas and 27-foot sidewalks on both sides (Section C-2). Farther east, Main Street transitions to 20-foot angled parking and 15-foot sidewalks (Section C-3), then transitions back to 8-foot parallel parking areas and 27-foot sidewalks on both sides (Section C-2) in the vicinity of Cherokee Trail.

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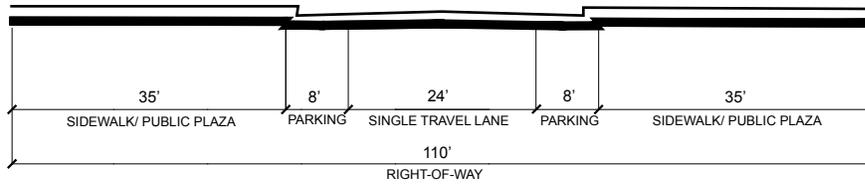
Section A-A Existing SR-62 Alignment



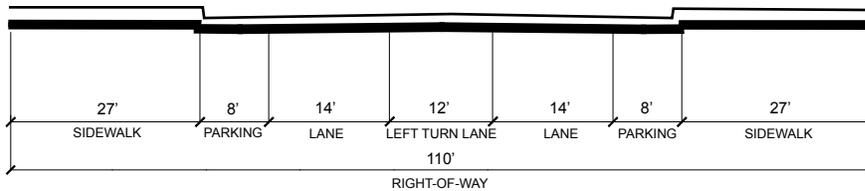
Section B-B Proposed SR-62 Realignment



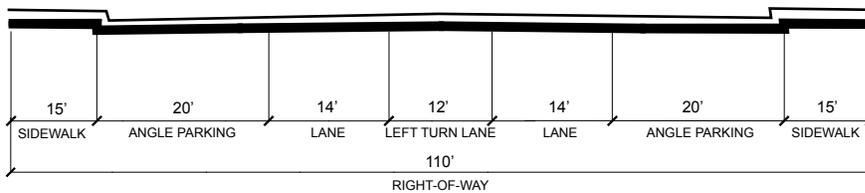
Section C1-C1 Mainstreet (Existing SR-62)



Section C2-C2 Main Street (Existing SR-62)



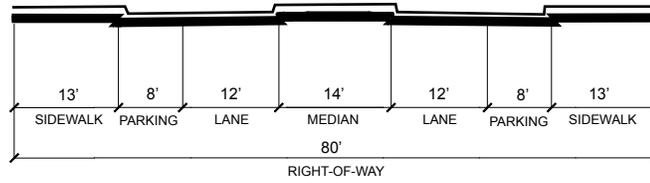
Section C3-C3 Main Street (Existing SR-62)



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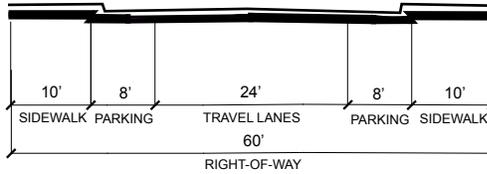
Section D-D

Primary Local Streets



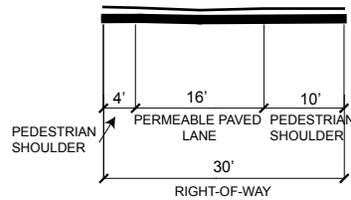
Section E-E

Secondary Local Streets



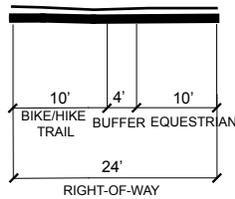
Section F-F

Alleys



Section G-G

Multi-Use Trail (per General Plan)



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Primary Local Streets (80' ROW)

The primary local streets are Kickapoo Trail, Santa Fe Trail, Fox Trail, Deer Trail, and Pioneer Trail. This cross-section includes an 80-foot ROW, one travel lane in each direction separated by a 14-foot median, with 8-foot parallel parking areas, and 13-foot sidewalks on both sides (Section D).

Secondary Local Streets (60' ROW)

Secondary local streets are Inca Trail, Geronimo Trail, Elk Trail, Cherokee Trail, Bannock Trail, Apache Trail, and Acoma Trail. This cross-section includes a 60-foot right-of-way, one travel lane in each direction, 8-foot parallel parking areas, and 10-foot sidewalks on both sides (Section E).

Alleys (30' ROW)

ROW alleys are located behind the Main Street, and are designed to promote pedestrian activity by incorporation of accented pedestrian shoulder paving and enhancement of the alleys by property owners. This cross-section includes a 30-foot ROW, one travel lane, and accented pedestrian paving on both sides of the alley (Section F).

3.3.3 SR-62 Realignment

The Old Town Yucca Valley Specific Plan incorporates the proposed SR-62 realignment to allow east-west traffic to travel around (instead of through) the Town, allowing Old Town Yucca Valley to become more pedestrian-oriented. Just east of Kickapoo Trail, SR-62 would turn northerly and transition to Yucca Trail in the vicinity of Hopi Trail. The ultimate realignment of SR-62 will be determined upon further study by Caltrans and the Town of Yucca Valley; the preferred realignment identified in this Specific Plan is conceptual. As part of the traffic analysis for this Specific Plan and discussions with the community and City Staff, Caltrans Alternative D was selected as the preferred SR-62 realignment alternative.

Analysis of Alternative D and Conclusions

The analysis of the SR-62 realignment alternatives was based on the forecast traffic volumes in the Town of Yucca Valley General Plan Program Circulation Element Traffic Study (Robert Kahn, John Kain and Associates, August 24, 1995), which assumes buildout of the Town of Yucca Valley General Plan. The level of service (LOS), based on the capacity of the average daily traffic (ADT) and the volume-to-capacity (V/C) ratio analysis utilized by the Town, was used to determine the functionality of a roadway segment. The operation goal for a roadway segment for the Town is LOS D or better (on a scale of LOS A to LOS F).

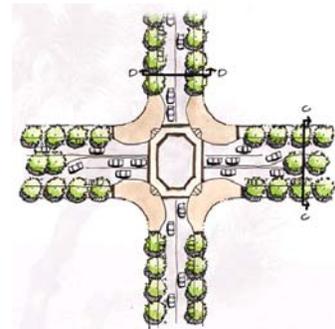
Development Plan

With the implementation of the proposed realignment of SR-62 (Alternative D), the existing alignment of SR-62 between Kickapoo Trail and Apache Trail would change classification to a two-lane divided industrial roadway and be redesignated as “Main Street.” Yucca Trail would be classified as a six-lane divided highway and designated as SR-62; Santa Fe Trail would remain classified as a four-lane divided collector.

With the forecast General Plan buildout and incorporation of the proposed SR-62 realignment (Alternative D), all of the study roadway segments are forecast to operate at an acceptable LOS.

3.3.4 Traffic Calming

The Old Town Yucca Specific Plan provides traffic-calming measures in the Circulation Plan that will slow traffic, reduce traffic noise, improve pedestrian safety, and so contribute to safe and walkable streets. Traffic-calming methods have been incorporated in the Main Street design, including mid block and corner bump outs, parallel and perpendicular parking areas, and enhanced intersection paving. Additional traffic-calming measures are encouraged to be utilized throughout the Specific Plan area, such as roundabouts, chokers, and raised medians.



Traffic Calming at Intersection

3.3.5 Pedestrian and Bicycle/Equestrian Trails

The trails system includes a pedestrian-oriented street system encompassing wide sidewalks and public plazas that vary between 15 to 35 feet along Main Street. In addition, 4-foot and 10-foot accent pedestrian shoulders are incorporated along alleys adjacent to Main Street. On-street Class 1 bike paths are proposed to extend along SR-62/Yucca Trail and Santa Fe Trail, connecting the local street network. The proposed Yucca Wash multi-use trail would be a 10-foot decomposed granite trail for equestrian and pedestrian use, ultimately connecting to the regional California Riding and Hiking Trail System.

3.4 Infrastructure Plan

This section describes the infrastructure improvements anticipated to meet the demands of the Land Use Plan. As this Specific Plan is a policy-level plan, note that the timing of all infrastructure improvements identified in this section represent the ultimate buildout conditions of the Old Town Yucca Valley Specific Plan. The Town of Yucca Valley will provide additional infrastructure information as it becomes available. The information referenced here was provided by the Town of Yucca Valley, based on reports available and/or developed during preparation of this Specific Plan. Refer to Appendix D, Support Information.

3.4.1 Domestic Water System

A Yucca Valley Revitalization Project Draft Utility Plan was prepared for the Specific Plan by RBF Consulting, dated September 9, 2005. This plan identifies the existing water demands, and compares the needs based upon buildout of the Specific Plan. The Specific Plan area is located within the 3495W Pressure Zone and is the supply zone that the west side wells pump directly into. The December 2001 High Desert Water District (HDWD) master plan identifies a need for additional water storage within this zone. The HDWD unit water usage factors and water demand factors adopted by other desert agencies are shown in Table 3-2, *HDWD Water Usage Factors*.

**Table 3-2
Water Usage Factors
(High Desert Water District)**

Land Use Type	High Desert Water District Duty Factor			Other Southern California Desert Regions*
	Acre Feet per Acre per Year	Gallons per Day per Acre	Gallons per Day per Dwelling Unit	
<i>Rural Residential</i>				
1 du/ac	0.3	-	-	0.75 – 3.36 AF/ac/yr
1 du/2.5 ac	0.15	-	-	0.75 – 3.36 AF/ac/yr
1 du/5 ac	0.08	-	-	0.75 – 3.36 AF/ac/yr
1 du/10, 20, 40 ac	0.03	-	-	0.75 – 2.35 AF/ac/yr
<i>Residential</i>				
1 du/ac	0.3	268	268	700 - 1500 gpd/du
2 du/ac	0.69	616	308	700 - 1000 gpd/du
2.5 du/ac	0.8	715	286	700 - 800 gpd/du
5 du/ac	1.1	983	197	400 - 540 gpd/du
<i>Multi-Family</i>	4.85	4330	216**	300 - 400 gpd/du
<i>Industrial/ Commercial</i>	0.26	232	-	1500 - 2000 gpd/ac

du = dwelling unit(s); ac = acre(s); gpd = gallons per day; AF = acre-feet; yr = year.

* Borrowed from Eastern Municipal Water District and Rancho California Water District guidelines.

** Assuming 20 dwelling units per acre.

The HDWD completed several miles of pipeline upgrades during the 1995-96 Pipeline Improvement Project, including the construction of 22,300 linear feet of replacement pipeline in the District's west side, which will directly benefit the 3495W Pressure Zone and the Specific Plan area; however, several older, and smaller (2-inch, 3-inch, and 4-inch), pipelines are still in operation; some of these still serve fire hydrants, which are sorely insufficient for providing even the lowest of current-day fire-flow requirements. Fire-flow criteria (as provided in the 2001

Water Master Plan Update) and appropriate system pipeline diameters are shown in Table 3-3, *Fire-Flow Pipe Dimensions*.

Table 3-3
Fire-Flow Pipe Dimensions
(High Desert Water District)

Land Use	Minimum Required Fire Flow (gallons per minute)	Minimum Pipe Diameter
Low-Density Residential	1,500	8 inches
Residential	2,000	8 inches (looping)
Commercial/Multi-family Residential	3,000	10 inches (looping)
Industrial	4,000	12 inches (looping)

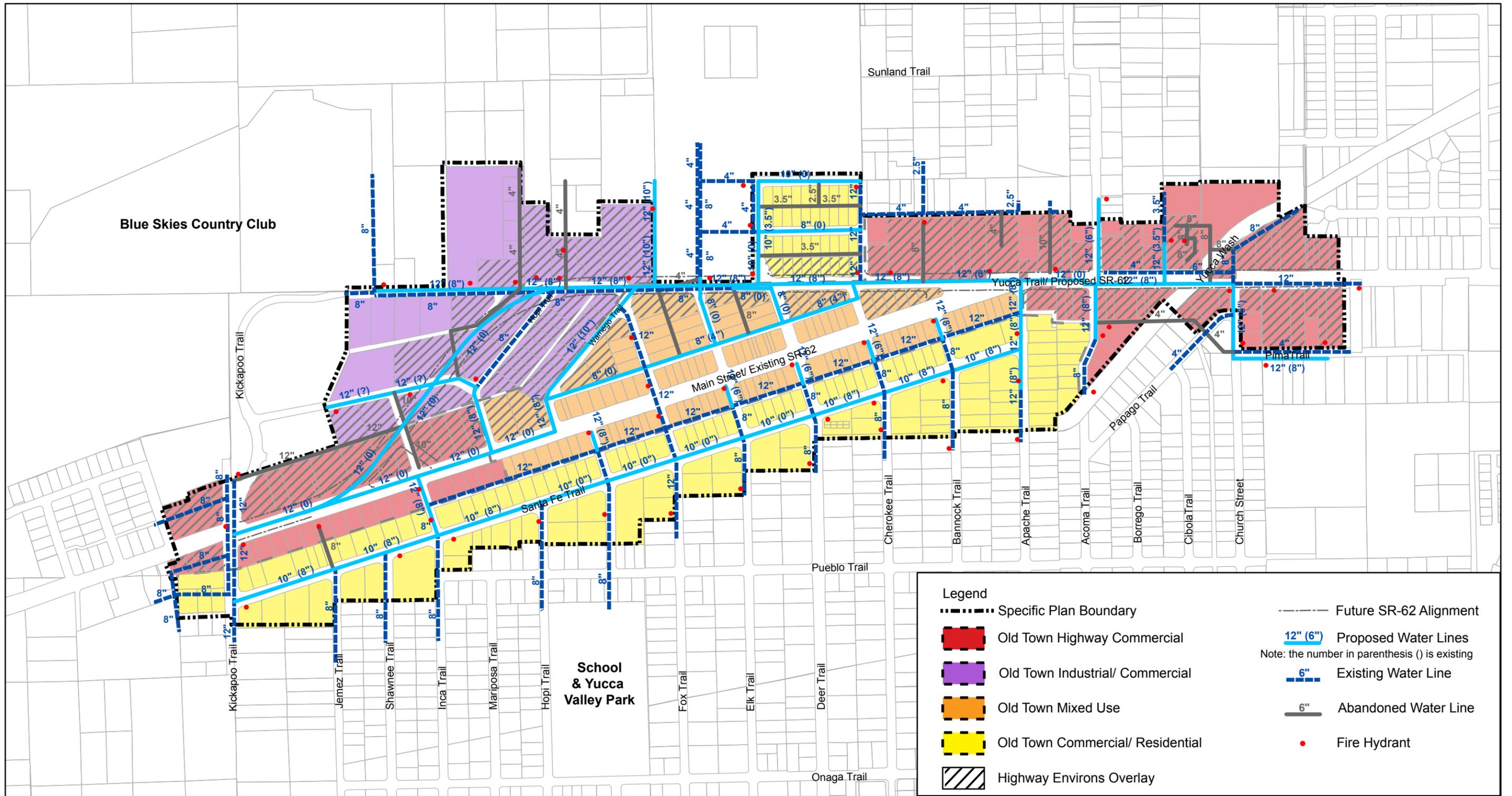
For the purposes of the Water Master Plan Study for the Specific Plan, the pipe diameter given here is based on the typical (and conservative) industry velocity standard of 10 feet per second (fps). This ensures a reasonable unit headloss within the system for maximum ability to provide the fire flows at the minimum residual pressure of 20 pounds per square inch (psi), as dictated by the Uniform Fire Code (UFC). Hydraulic analysis should be performed at the design phase of the project to verify that these pipe diameters work within the operation of the HDWD transmission system as a whole.

In 1997, the HDWD began a program to replace old and undersized pipelines. This study assumes that all replacement projects completed to date are incorporated into the 2002 Water System Atlas. Refer to Table 3-4, *Water Pipeline Replacements Completed*.

Table 3-4
Water Pipeline Replacements Completed
(High Desert Water District)

Fiscal Year of Construction	Area	Lineal Footage Installed
2000/2001	Jemez Trail and Highland Trail, between Kickapoo Trail and Inca Trail	1,500
2000/2001	Inca Trail and Mariposa Trail, between Mariposa and Fox Trail and between Yucca Trail and 29 Palms Highway	2,300
2002/2003	Coyote Trail and Apache Trail, north of 29 Palms Highway	3,400

The Specific Plan will result in the buildout of water infrastructure and presents an opportunity to upgrade and ensure the adequacy of fire hydrant coverage. In locations that cannot be reached by conventional fire department equipment from existing public fire hydrants, new fire hydrants may be added and/or old hydrants replaced/relocated as part of the infrastructure upgrades. The proposed water system upgrades require prior verification through computer model simulation. Refer to Table 3-5, *Proposed Water Infrastructure Improvements*, and Exhibit 3-4, *Proposed Water Plan*.



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Table 3-5
Proposed Water Infrastructure Improvements
(High Desert Water District)

Proposed Improvement		Approximate Length (feet)
1	Replace existing 3.5" steel pipe with 8" PVC pipe	320
2	Replace existing 4" steel pipe with 8" PVC pipe	1,300
3	Replace existing 3.5" steel pipe with 10" PVC pipe	260
4	Replace existing 6" steel/PVC pipe with 10" PVC pipe	1,425
5	Replace existing 8" steel/PVC pipe with 10" PVC pipe	855
6	Replace existing 8" steel/PVC pipe with 12" PVC pipe	5,055
7	Replace existing 10" steel/PVC pipe with 12" PVC pipe	860
8	Replace existing unknown pipe with 12" PVC pipe	700
9	Install new 8" PVC Pipe	4,770
10	Install new 12" PVC Pipe	3,755
11	Abandon 2.5" steel pipe in ROW	160
12	Abandon 3.5" PVC/steel pipe in ROW	700
13	Abandon 4" steel pipe in ROW	2,495
14	Abandon 8" steel pipe in ROW	1,650
15	Abandon 10" steel pipe in ROW	905
16	Abandon 12" ACP pipe in ROW	1,150
17	Abandon "Unknown" PVC pipe in ROW	1,840

PVC = polyvinyl chloride; ROW = right-of-way; ACP =Asbestos Cement Pipe

Storage

The 1995 and 2001 Water Master Plans define water storage requirements due to three separate needs – operational, emergency, and fire. Both the 1995 and 2001 Master Plans discuss the need for additional storage in the 3495W Pressure Zone. The *2001 HDWD Water Master Plan Update* (Section VII) describes additional storage capacity needs based on the 2001 storage capacity of 4.5 million gallons (MG). Projected water demands for the 3495W Pressure Zone (both east and west sides) produce a need for 4.72 MG for 2005, and 5.57 MG for 2020, according to Tables VII-1B and VII-1A of the Update. This represents an additional storage need for the 3495W Zone, as a whole, of approximately 0.2 MG in 2005 and 1.1 MG in 2020. Current storage capacity in the 3495W Zone may be adequate for the additional demands estimated from the Project.

3.4.2 Wastewater System

It is anticipated that private septic systems will continue to be used for wastewater disposal until sufficient development has occurred to extend sewer system infrastructure to Yucca Valley. Septic tank discharges have contaminated some of the groundwater supply with high nitrate levels. The maximum nitrate contaminant level allowed by the Yucca Valley General Plan Water Resources Element is 45 milligrams per liter (mg/L). Tests performed in 1992 showed nitrate measurements ranging between 2.9 and 24.1 mg/L; however, as of this writing, no test results have been provided since then. Therefore, additional well testing is recommended.

The High Desert Water District anticipates constructing a wastewater treatment plant in Yucca Valley, northeast of the Specific Plan area. Future wastewater improvements, including the elimination of private septic systems and the construction of new wastewater collection, treatment, and disposal systems, will require a coordinated effort between the Town of Yucca Valley and the High Desert Water District.

3.4.3 Storm Drainage System

The Town of Yucca Valley Master Plan of Drainage states that most of the stormwater flows from the eastern portion of the area will be conveyed through Yucca Wash; storm flows from the southern portions will be conveyed through street flows within Santa Fe Trail, southwest to the intersection of Inca Trail and Santa Fe Trail. From that intersection, stormwater will be conveyed through a reinforced concrete pipe to the Blue Skies Country Club. Refer to Exhibit 3-5, *Existing and Proposed Drainage Plan*.

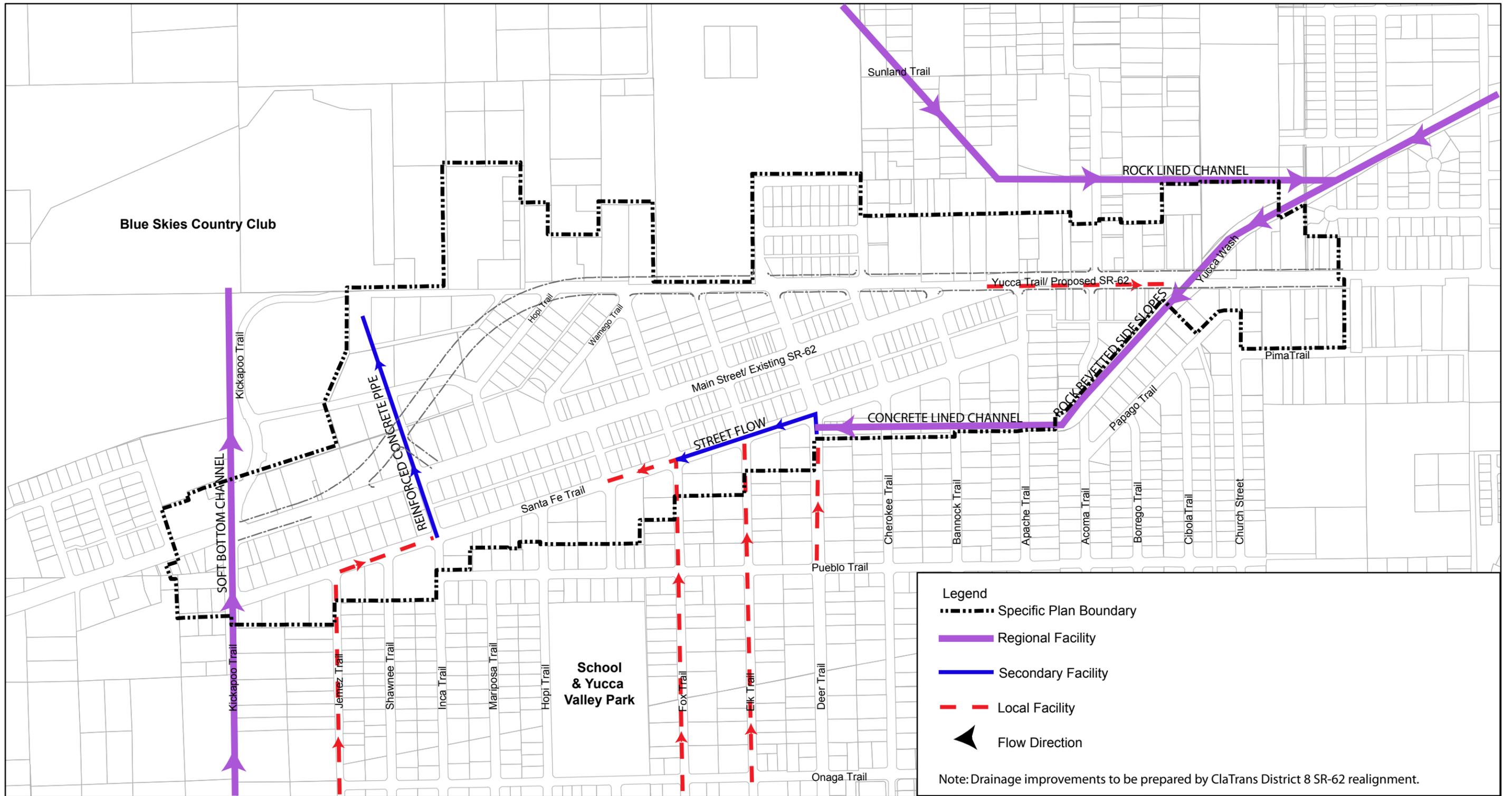
Yucca Wash northeast of Apache Trail is proposed to be constructed of rock-revetted side slopes, and the southwest portion will be a concrete-lined channel. Stormwater flows will be conveyed via curb street systems.

3.5 Public Services

This section describes the public services provided in the Specific Plan area and their ability to meet the new demand in the ultimate buildout conditions of the Old Town Yucca Valley Specific Plan. Information referenced is provided by the Town of Yucca Valley and is based on information available and/or prepared at the time of preparation of this Specific Plan.

3.5.1 Schools

The Morongo Unified School District (MUSD) provides public education to the residents of the Morongo Basin, which includes the Town of Yucca Valley. The public schools in the Specific Plan area are Yucca Valley Elementary School, La Contenta Junior High School, and Yucca Valley High School.



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3.5.2 Fire Protection

The San Bernardino County Fire Department provides fire protection to the Town of Yucca Valley. The Department's Yucca Valley Fire District encompasses 61 square miles and has two fire stations. The closest fire station to the Specific Plan area is Station #121, located at 55201 29 Palms Highway.

3.5.3 Law Enforcement

Police protection is provided by contract with the San Bernardino County Sheriff's Department, located at 6527 White Feather Road in Joshua Tree.

3.5.4 Library

The Yucca Valley Library, a branch of the San Bernardino County Library System, is located at the Community Center Complex. The San Bernardino County Library Master Facility Master Plan expresses the need to expand this library or to move it to a new facility to accommodate the increasing population in Yucca Valley.

3.5.5 Communication Systems

Residential and commercial telephone service is provided by GTE California, headquartered in Thousand Oaks. Century Communication provides cable television.

3.5.6 Electricity

Southern California Edison (SCE) provides electricity in the Specific Plan area. Additional hook-ups are feasible.

3.5.7 Natural Gas

The Southern California Gas Company provides natural gas service to the Specific Plan area. Additional hook-ups are feasible.

3.5.8 Solid Waste

Hi-Desert Disposal provides solid waste collection and disposal services for both businesses and residences. Recycling is provided by Hi-Desert Recycling at a recycling center.

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Chapter

4

DEVELOPMENT
REGULATIONS

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CHAPTER 4: DEVELOPMENT REGULATIONS

4.0 Purpose and Intent

This chapter establishes the zoning districts for the Yucca Valley Old Town Specific Plan area, the allowable uses (allowed and conditionally allowed) that apply within each zoning district, and the development and design standards that apply within each district. Together, the table of allowed uses and the development and design standards prescribe the allowed development for the area.

The intent of the Land Use and Development Regulations, together with the Design Guidelines is to implement the goals of the Specific Plan. The land use regulations, development standards, and design standards are consistent with the goals and policies of the General Plan.

4.0.1 Organization of this Chapter

- 4.1 General Provisions
- 4.2 Establishment of Land Use Districts
- 4.3 Allowable Land Uses and Permit Requirements
- 4.4 Nonconforming Uses, Structures and Parcels
- 4.5 Zoning District Development Standards
- 4.6 Standards for Specific Land Uses
- 4.7 Off-Street Parking and Loading Standards
- 4.8 Landscaping, Walls and Fences
- 4.9 Signs
- 4.10 Auxiliary Structures, Equipment, and Utilities
- 4.11 General Operating Standards
- 4.12 Development Incentives
- 4.13 Development Review Procedures
- 4.14 Highway Environs Overlay District

Development Regulations

4.1 General Provisions

4.1.1 Minimum Requirements

The land use and development standards contained herein are minimum requirements. In reviewing individual projects requiring discretionary approval, more restrictive standards or conditions may be applied by the Town of Yucca Valley to accomplish the goals and objectives of this Specific Plan.

4.1.2 Applicability of Development Standards and Guidelines

The land use and development standards contained in this Chapter 4 shall apply to parcels within the Specific Plan's project area boundaries. All new development projects, including additions to buildings and changes in use on a parcel, are subject to the provisions of this chapter.

4.1.3 Interpretation, Administration, Enforcement, and Appeals

The Department Director is authorized by the Town Council to interpret, administer, and enforce the provisions of this chapter. The Department Director may designate a representative to act on his or her behalf.

The provisions of this chapter shall be interpreted in a manner that best fulfills the spirit and intent of the Specific Plan. The Department Director shall interpret questions arising from the application of this Chapter. A decision or determination of the Department Director may be appealed in compliance with the appeals procedures set forth in this Specific Plan in addition to the provisions of the Yucca Valley Municipal Code.

4.2 Establishment of Land Use Districts

4.2.1 Purposes of Land Use Districts

Section A describes the purpose and intent of each of the land use districts in the Old Town Yucca Valley Specific Plan.

1. **(OTMU) Old Town Mixed-Use District.** The Old Town Mixed-Use District is intended to serve as the core downtown district of the Specific Plan and is envisioned to provide a complementary mix of land uses and development types that are compatible with and reinforce pedestrian activity, interaction, and convenient access. The OTMU District is envisioned to unify the area and establish a core downtown environment providing a complementary mix of residential, commercial, and smaller-scale, street-oriented retail developments. This land use district encourages quality design, compact and vertically mixed development, higher residential densities and development intensities.

2. **(OTHC) Old Town Highway Commercial District.** The State Route 62 Highway Commercial District is envisioned to serve as the primary Eastern and Western Gateways to the Old Town Specific Plan through a highway-oriented commercial development that caters to the local and regional markets. The land use district is intended to provide a wide range of retail sales, business uses, and personal services oriented to the automobile customer. The OTHC District is envisioned as a primary node for serving the general commercial needs of the Town by promoting stable and attractive retail development.
3. **(OTCR) Downtown Commercial/Residential District.** The Downtown Commercial/Residential District is intended to provide a complementary mix of commercial and residential development. The district is a buffer between higher-intensity development in the OTMU District and adjacent residential development south of the Specific Plan project area. High-quality, well-designed commercial, office, and residential uses are encouraged within the OTCR District.
4. **(OTIC) Old Town Industrial/Commercial District.** The State Route 62 Industrial/Commercial District is planned to provide a variety of industrial, light industrial, flex-tech, service commercial, and limited live-work residential development. The OTIC District encourages quality-designed, lower-intensity industrial development of smaller-scale manufacturing and assembly. The District is intended to serve as a primary node for locally serving businesses and commercial activity. By establishing streetscape elements, landscaped buffers, and quality site design, the OTIC District is envisioned to contribute to a positive visual image along State Route 62.
5. **(HE) Highway Environs Overlay District.** The intent of the State Route 62 Highway Environs Overlay District is to provide a heightened level of discretionary review for development proposals in areas where changes in land use regulations are contemplated by the potential future realignment of State Route 62 (SR-62). The HE Overlay District ensures that future development proposals are not adversely affected by the realignment of SR-62.

4.3 Allowable Land Uses and Permit Requirements

Table 4-1 identifies the allowable land uses for each land use district that is exclusive to the Old Town Specific Plan: OTMU (Old Town Mixed-Use) district, OTHC (Old Town Highway Commercial), OTCR (Old Town Commercial/Residential), OTIC (Old Town Industrial/Commercial) and HE (Highway Environs Overlay). To determine the allowed land uses and development standards in the zones that are not exclusive to the Old Town Specific Plan, refer to applicable sections of the Yucca Valley Municipal Code.

4.3.1 Allowed Uses.

1. **Allowed Land Uses.** Table 4-1 identifies the uses of land allowed by this Specific Plan, and the land use permit required to establish each use.

Development Regulations

2. **Prohibited Land Uses.** Any table cell indicated a “-” symbol indicates that the listed land use is prohibited in that specific land use district.
3. **Applicable Sections.** Wherever the last column in the tables ("See Standards in Section") includes a Specific Plan section number, the regulations in the referenced section apply to the use; however, provisions in other sections of this Specific Plan or applicable sections of the Yucca Valley Municipal Code may also apply.

**TABLE 4-1
ALLOWABLE USES AND PERMIT REQUIREMENTS FOR DISTRICTS FOR OLD TOWN YUCCA VALLEY SPECIFIC PLAN**

Land Use	Permit Requirement by District						Notes:
	OTMU	OTHC	OTCR	OTIC	HE Overlay		
Key to Table SPR Site Plan Review is Required. P Use is Allowed (Refer to Development Review Procedures section) CUP Conditional Use Permit is required.							
OTMU= Old Town Mixed-Use OTHC= Old Town Highway Commercial OTCR= Old Town Commercial/Residential OTIC= Old Town Industrial/Commercial HE= Highway Environs (SR-62)							
Miscellaneous							
Alternative Fuels and Recharging Facilities	P		P		SPR		
Broadcast and Recording Studios	P		P		SPR		
Motor Vehicle Storage Facilities				P	SPR		
Public Utility Service Yards	P			P	SPR		
Public Utility Structures and Service Facilities	P			P	SPR		
Public Works Maintenance Facilities and Storage Yards				P	SPR		
Satellite Dishes/Antennas (less than 3 feet/2meters in diameter)	P	P	P	P	SPR		
Education							
Community/Cultural Centers	P		P		SPR		
Commercial Day Care Centers	P		P		SPR		
Membership Organization Facilities – Lodges and Clubs	P		P		SPR		
Schools – K-12, Private	P		P		SPR		

Development Regulations

**TABLE 4-1
ALLOWABLE USES AND PERMIT REQUIREMENTS FOR DISTRICTS FOR OLD TOWN YUCCA VALLEY SPECIFIC PLAN**

Land Use	Permit Requirement by District						Notes:
	OTMU	OTHC	OTCR	OTIC	HE Overlay		
Key to Table		OTMU= Old Town Mixed-Use OTHC= Old Town Highway Commercial OTCR= Old Town Commercial/Residential OTIC= Old Town Industrial/Commercial HE= Highway Environs (SR-62)					
SPR	Site Plan Review is Required.						
P	Use is Allowed (Refer to Development Review Procedures section)						
CUP	Conditional Use Permit is required.						
Schools, Specialized Education and Training – less than 50 students	P		P	P	SPR		
Schools, Specialized Education and Training – more than 50 students			P	P	SPR		
Studios — Art, Dance, Music Photography, etc.	P		P	P	SPR		
Universities/Colleges, Private	P	P	P	P	SPR		
Entertainment, Recreation, Public Assembly							
Adult-Oriented	Refer to Adult Business Ordinance						
Dancing and Live Entertainment	CUP	CUP			SPR		
Health /Fitness Centers	P	P	P	P	SPR		
Indoor Amusement/ Entertainment/ Recreation Centers/ Arcades	P				SPR		
Nightclubs	CUP				SPR		Accessory to restaurant use only
Places of Worship	P	P	P	P	SPR		
Theaters, Auditoriums, and Meeting Halls	P	P	P		SPR		
Manufacturing and Processing							
Chemical Products				P	SPR		

**TABLE 4-1
ALLOWABLE USES AND PERMIT REQUIREMENTS FOR DISTRICTS FOR OLD TOWN YUCCA VALLEY SPECIFIC PLAN**

Land Use	Permit Requirement by District						Notes:
	OTMU	OTHC	OTCR	OTIC	HE Overlay		
Clothing Products				P	SPR		
Cosmetics				P	SPR		
Electronics and Equipment Manufacturing				P	SPR		
Foam/Plastics Fabrication				P	SPR		
Food and Beverage Product Manufacturing				P	SPR		
Furniture/Cabinet Shops				P	SPR		
Glass Products				P	SPR		
Handicraft Industries, Small Scale Assembly (premanufactured)				P	SPR		
Hazardous Material Storage				P	SPR		
Laboratories			P	P	SPR		
Metal Products Fabrication				P	SPR		
Paper Products				P	SPR		
Pharmaceutical Manufacturing				P	SPR		
Plastics and Rubber Products				P	SPR		
Printing and Publishing	P		P	P	SPR		
Recycling Facilities – Small Collection Facility	P		P	P	SPR		

Key to Table
SPR Site Plan Review is Required.
P Use is Allowed (Refer to Development Review Procedures section)
CUP Conditional Use Permit is required.

OTMU= Old Town Mixed-Use
 OTHC= Old Town Highway Commercial
 OTCR= Old Town Commercial/Residential
 OTIC= Old Town Industrial/Commercial
 HE= Highway Environs (SR-62)

Development Regulations

**TABLE 4-1
ALLOWABLE USES AND PERMIT REQUIREMENTS FOR DISTRICTS FOR OLD TOWN YUCCA VALLEY SPECIFIC PLAN**

Land Use	Permit Requirement by District						Notes:
	OTMU	OTHC	OTCR	OTIC	HE Overlay		
Recycling Facilities – Reverse Vending Machines	P		P	P	SPR		
Research and Development	P		P	P	SPR		
Structural Clay and Pottery Products				P	SPR		
Textile Products				P	SPR		
Warehousing, Wholesaling, and Distribution Facility, Incidental (less than 75% of floor area)				P	SPR		
Motor Vehicle and Related Retail Trade and Services							
Car Wash, Full & Self-Service		P		P	SPR		
Marine Sales, Indoor	P	P	P	P	SPR		
Motor Vehicle Leasing, No Onsite Storage	P	P	P	P	SPR		
Motor Vehicle Leasing, Onsite Vehicle Storage (less than 10 vehicles)		P	P	P	SPR		
Motor Vehicle Parts and Supplies Sales (no onsite repairs)	P	P		P	SPR		

**TABLE 4-1
ALLOWABLE USES AND PERMIT REQUIREMENTS FOR DISTRICTS FOR OLD TOWN YUCCA VALLEY SPECIFIC PLAN**

Land Use	Permit Requirement by District						Notes:
	OTMU	OTHC	OTCR	OTIC	HE Overlay		
Automated Car Washing		P			SPR		
Motor Vehicle Repair and Maintenance, MAJOR, and only when conducted in an enclosed structure.		P		P	SPR		
Motor Vehicle Repair and Maintenance, MINOR, and only when conducted in an enclosed structure.		P		P	SPR		
Motor Vehicle Sales (New and/or Used), With Service Facilities		P		P	SPR		
Motor Vehicle Sales (New and/or Used), Without Service Facilities, Storage or Outdoor Display	P	P	P	P	SPR		
Motor Vehicle Window Tinting		P		P	SPR		
Service Stations		P		P	SPR		
Public							
Libraries/ Museums, Public	P		P		SPR		
Parking Lots/Structures, Public	P	P	P	P	SPR		
Parks, Public	P	P	P	P	SPR		

Development Regulations

**TABLE 4-1
ALLOWABLE USES AND PERMIT REQUIREMENTS FOR DISTRICTS FOR OLD TOWN YUCCA VALLEY SPECIFIC PLAN**

Land Use	Permit Requirement by District						Notes:
	OTMU	OTHC	OTCR	OTIC	HE Overlay		
Service Uses/ Structures, Public			P		SPR		
Retail Trade							
Accessory Retail Uses	P	P	P	P	SPR		
Art, Antiques, Collectibles, and Gifts	P	P	P	P	SPR		
Bakeries, Retail	P	P		P	SPR		
Book Stores	P	P		P	SPR		
Building Material Stores/Yards		P		P	SPR		
Candy Stores	P	P		P	SPR		
Catering Businesses, ONLY When Ancillary to a Restaurant	P	P		P	SPR		
Convenience Markets/Stores (under 5,000 square feet)	P	P		P	SPR		
Furniture, Furnishings and Equipment Stores	P	P		P	SPR		
Garden Centers/Plant Nurseries	P	P		P	SPR		
Grocery Stores, 5,000 sq. ft. and greater		P			SPR		
Liquor Sales (On-site Consumption Only) - Restaurant	P	P			SPR		

**TABLE 4-1
ALLOWABLE USES AND PERMIT REQUIREMENTS FOR DISTRICTS FOR OLD TOWN YUCCA VALLEY SPECIFIC PLAN**

Land Use	Permit Requirement by District						Notes:
	OTMU	OTHC	OTCR	OTIC	HE Overlay		
Liquor Sales (Off-site Consumption Only)	P	P			SPR		
Liquor Sales (Off-site Consumption Only) – Beer and Wine Only	P	P			SPR		
Meat Markets or Delicatessens	P	P			SPR		
Music Stores	P	P			SPR		
Pet Stores	P	P			SPR		
Pharmacies/Drug Stores	P	P			SPR		
Pharmacies/Drug Stores, Drive-thru Service		P			SPR		
Restaurants, Fast Food, Drive-thru Service		P			SPR		
Restaurants, Fast Food, Without Drive-thru Service	P	P			SPR		
Restaurants or Cafes (excluding Fast Food or Drive-ins)	P	P			SPR		
Retail Stores, General Merchandise	P	P			SPR		
Shopping Centers (five or more tenants on contiguous parcel)	P	P			SPR		
Temporary Uses/Activities							

Key to Table
SPR Site Plan Review is Required.
P Use is Allowed (Refer to Development Review Procedures section)
CUP Conditional Use Permit is required.

OTMU= Old Town Mixed-Use
 OTHC= Old Town Highway Commercial
 OTCR= Old Town Commercial/Residential
 OTIC= Old Town Industrial/Commercial
 HE= Highway Environs (SR-62)

Development Regulations

**TABLE 4-1
ALLOWABLE USES AND PERMIT REQUIREMENTS FOR DISTRICTS FOR OLD TOWN YUCCA VALLEY SPECIFIC PLAN**

Land Use	Permit Requirement by District						Notes:
	OTMU	OTHC	OTCR	OTIC	HE Overlay		
Big Box Retail Stores (greater than 30,000 square feet)		P			SPR		
Services							
Automated Teller Machines (ATMs)	P		P	P	SPR		
Banks and Financial Services	P	P	P	P	SPR		
Business Support Services, Secretarial and Administrative	P	P	P	P	SPR		
Check Cashing	P	P		P	SPR		
Contractor's Equipment Yards				P	SPR		
Convalescent/Rest Homes			P		SPR		
Equipment Rental Establishments				P	SPR		
Hotels/Motels	P	P			SPR		
Laundromats, Self-Service and Dry Cleaning, Drop-off only	P	P	CUP	CUP	SPR		
Massage Therapy	Refer to Title 8 of the Yucca Valley Municipal Code						
Medical Services, Clinics	P	P	P		SPR		
Medical Services, Laboratories	P		P	P	SPR		
Offices, Professional	P		P	P	SPR		

Key to Table
SPR Site Plan Review is Required.
P Use is Allowed (Refer to Development Review Procedures section)
CUP Conditional Use Permit is required.

OTMU= Old Town Mixed-Use
 OTHC= Old Town Highway Commercial
 OTCR= Old Town Commercial/Residential
 OTIC= Old Town Industrial/Commercial
 HE= Highway Environs (SR-62)

**TABLE 4-1
ALLOWABLE USES AND PERMIT REQUIREMENTS FOR DISTRICTS FOR OLD TOWN YUCCA VALLEY SPECIFIC PLAN**

Land Use	Permit Requirement by District						Notes:
	OTMU	OTHC	OTCR	OTIC	HE Overlay		
Outdoor Active Activities (e.g., Walk-up Windows)	P				SPR		
Personal Services, General	P		P	P	SPR		
Photocopy/Printing/Desktop Facilities	P		P	P	SPR		
Printing/Photo Developing/Printing Shops	P		P	P	SPR		
Photography Studio/Supply Shop	P		P	P	SPR		
Propane/Liquid Fuel (Storage and Sales)				P	SPR		
Repair and Maintenance, Consumer Products	P		P	P	SPR		
Storage (Mini, Personal, and Self-Storage) Facilities		CUP		P	SPR		
Tanning Studios/Salons	P	P			SPR		
Travel Agencies	P	P	P	P	SPR		
Veterinarian Clinics and Animal Hospitals	P		P	P	SPR		
Residential							
Accessory Uses and Structures	P	P	P	P	SPR		
Multiple Family Residential	P		P	P	SPR		

Key to Table
SPR Site Plan Review is Required.
P Use is Allowed (Refer to Development Review Procedures section)
CUP Conditional Use Permit is required.

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Development Regulations

**TABLE 4-1
ALLOWABLE USES AND PERMIT REQUIREMENTS FOR DISTRICTS FOR OLD TOWN YUCCA VALLEY SPECIFIC PLAN**

Land Use	Permit Requirement by District						Notes:
	OTMU	OTHC	OTCR	OTIC	HE Overlay		
Assisted Living Facilities			P	P	SPR		
Condominiums/Townhomes	P		P	P	SPR		
Live-Work Facilities	P		P	P	SPR		
Mixed-Use Developments (Residential over Retail/Office)	P	P	P	P	SPR		
Mixed-Use Developments (Use distributed horizontally on a site)	P	P	P	CUP	SPR		
Residential Care Homes			P		SPR		

Key to Table
SPR Site Plan Review is Required.
P Use is Allowed (Refer to Development Review Procedures section)
CUP Conditional Use Permit is required.

OTMU= Old Town Mixed-Use
 OTHC= Old Town Highway Commercial
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4.4 Nonconforming Uses, Structures, and Parcels

4.4.1 Purpose and Intent

This section establishes uniform provisions for the regulation of legal nonconforming uses and structures. Within the zoning districts established by this Specific Plan, certain land uses and structures were legally established under the requirements of the Yucca Valley Municipal Code that may be prohibited, regulated, or restricted differently under the terms of this Specific Plan or any subsequent amendments of it.

This Specific Plan discourages the long-term continuance of these nonconformities and encourages their abatement. However, this Specific Plan has provisions that permit these nonconforming uses to be expanded or redeveloped if certain conditions are met.

Nonconforming uses and structures shall not be enlarged, expanded, or extended, except as expressly stated in this section, nor shall the existence of a nonconforming use or structure be a determining factor for adding other uses or structures allowed in the same land use district.

4.4.2 Nonconforming Uses

A nonconforming use shall be subject to the provisions contained in Title 8, Division 4, Chapter 8, of the Yucca Valley Municipal Code, in addition to the following standards:

1. A legal nonconforming use may be altered, subject to the approval by the Department Director or Development Review Committee. The Department Director shall have the discretion to refer to the Planning Commission uses that are expanded or modified by greater than twenty-five percent (25%) of the floor space or ground area existing at the time the use became nonconforming.
2. For a nonconforming use of land not involving a structure, the use may not be moved, in whole or in part, from the location it occupied at the time the use became nonconforming to any other portion of the lot or parcel on which it is located.
3. If no structural alterations are made, a nonconforming use of a structure shall be changed to another nonconforming use, provided the Department Director determines that the proposed use is equally or more appropriate to the land use district than the existing nonconforming use. In permitting such a change, the Department Director may impose conditions to ensure that the degree of nonconformity is not subsequently increased, along with such other conditions as are necessary to mitigate any adverse impacts of the changed use upon neighboring properties.

Development Regulations

4. If a nonconforming use is superseded by an allowed use, such use shall thereafter conform to the regulations for the land use district in which it is located, and the nonconforming use may not be resumed.

4.4.3 Nonconforming Structures

A nonconforming structure may be continued in use so long as it is in compliance with all laws and other than building code regulations as set forth in the Yucca Valley Municipal Code, subject to the following provisions;

1. If a nonconforming structure or nonconforming portion of a structure is destroyed by any means to an extent of more than seventy-five percent (75%) of its reasonable replacement value at the time of destruction, it may not be reconstructed except in conformity with the provisions of this Specific Plan.
 - Exception: A nonconforming residential structure may be reconstructed in any district that allows such a use (i.e., OTMU and OTCR) if it meets all building and safety regulations, and if, to the extent reasonable and feasible, the construction conforms to the regulations of this Specific Plan.
2. If a structure is moved for any reason for any distance, it shall thereafter conform to the regulations for the land use district into which it is relocated.
3. If a nonconforming structure is abandoned or vacated for one-hundred eighty days or more, any subsequent use of the property shall conform to the regulations of the land use district in which it is located.

4.4.4 Nonconforming Parcels

Any lot that becomes nonconforming upon adoption of this Specific Plan may be used or built upon, provided that the development project otherwise conforms to the requirements of this Specific Plan.

Any lot that, by action of the Town of Yucca Valley or any other government agency, becomes nonconforming by means of condemnation, acquisition, or dedication shall be allowed to apply the standards for the lot in effect prior to the agency action.

4.4.5 Repairs and Maintenance of Nonconformities

On any nonconforming structure, and on any conforming structure containing a nonconforming use, routine maintenance work may be performed and nonbearing walls, fixtures, wiring, or plumbing may be repaired or replaced; provided that:

- The value of the repair and replacement work during any twelve-month period does not exceed ten percent of the replacement cost of the whole structure; and

- The square footage of the structure that is nonconforming or has a nonconforming use is not increased.

4.5 Zoning District Development Standards

New land uses and structures, and alterations to existing land uses and structures, shall be designated, constructed, and/or established in compliance with the requirements in *Table 4-2*, below.

Table 4-2
General Development Standards and
Requirements for Old Town Yucca Valley Districts

Development Feature	Old Town Mixed-Use (OTMU)	Old Town Highway Commercial (OTHC)
Floor-to-Floor Area Ratio (FAR)	Up to 1.0	0.35
Dwelling Units per acre (du/ac)	Up to 40	–
Minimum Setbacks Required:		
Front (Building)	0 feet	15 feet
Front (Parking)	N/A	10 feet (minimum landscape setback when parking is located at the front setback)
Side (each)	0 feet; when abutting an alley, 5 feet; when abutting residential 10 feet, plus 5 feet for each story above two	10 feet
Street Side	0 feet	15 feet
Rear	10 feet; when abutting an alley, 5 feet; when abutting residential, 20 feet plus 5 feet for each story above two	10 feet; when abutting residential, 20 feet plus 5 feet for each story above two
Minimum Street Frontage	50 feet	50 feet
Maximum Height	3 stories or 45 feet, whichever is less	40 feet
Accessory Structures	See Section 4.10	
Landscaping	See Section 4.8	
Parking	See Section 4.7	
Signs	See Section 4.9	

Development Regulations

Table 4-2 (cont'd)
General Development Standards
Requirements by Individual Zoning District

Development Feature	Old Town Commercial/Residential (OTCR)	Old Town Industrial/Commercial (OTIC)
Floor Area Ratio	0.40	0.40
Dwelling Units (du/ac)	24	30 ¹
Minimum Setbacks:		
Front (Building)	15 feet	15 feet
Front (Parking)	N/A	10 feet (minimum landscape setback when parking is located at the front setback)
Side (each)	0 feet; when abutting an alley, 5 feet; when abutting residential 10 feet, plus 5 feet for each story above two	0 feet; when abutting residential 10 feet, plus 5 feet for each story above two
Street Side	0 feet;	15 feet
Rear	10 ft; when abutting an alley, 5 feet; when abutting residential, 20 feet plus 5 feet for every story above two	10 feet; when abutting residential, 20 feet plus 5 feet for every story above two
Minimum Street Frontage	50 feet	50 feet
Maximum Height	3 stories or 45 feet, whichever is less	3 stories or 45 feet, whichever is less
Accessory Structures	See Section 4.10	
Landscaping	See Section 4.8	
Parking	See Section 4.7	
Signs	See Section 4.9	

Table 4-2 (cont'd)
General Development Standards
Requirements by Individual Zoning District

Development Feature	HE Highway Environs Overlay
Floor Area Ratio	Refer to Section 4.14
Dwelling Units (du/ac)	Refer to Section 4.14
Minimum Setbacks:	
Front (Building)	Refer to Section 4.14
Front (Parking)	Refer to Section 4.14
Side (each)	Refer to Section 4.14
Street side	Refer to Section 4.14
Rear	Refer to Section 4.14
Minimum Street Frontage	Refer to Section 4.14
Maximum Height	Refer to Section 4.14
Accessory Structures	See Section 4.10
Landscaping	See Section 4.8
Parking	See Section 4.7
Signs	See Section 4.9

4.5.1 Limitations and Exceptions to Allowed Uses and Structures

1. **Limitation on Storage.** No material or equipment shall be stored within the space between a street and setback line except temporarily during construction on the same parcel. No required setback area shall be used for storage of any motorized vehicle, mechanical equipment, or parts thereof, or any type of antenna except as provided in this Specific Plan.
2. **Limitation on Outdoor Uses.**
 - a. All uses shall be conducted entirely within a completely enclosed building that is attached to a permanent foundation.
 - Exceptions: Outdoor eating, temporary retail sales, vehicle sales and rental, parking, growing plants, cut flowers, Christmas tree and pumpkin lots, and similar uses, consistent with the Yucca Valley Municipal Code.
 - b. Unless screened from view consistent with Yucca Valley Municipal Code, the outside storage of equipment, materials, supplies, or tools is prohibited.

Development Regulations

- c. All uses shall be conducted in a manner that is not reasonably objectionable to a person of normal sensitivity by reason of dust, fumes, noise, odor, smoke, vibrations, or other similar causes.

4.6 Standards for Specific Land Uses

4.6.1 Equipment Rental Uses

Equipment rental and leasing businesses are allowed in compliance with Table 4-2 of this chapter, and shall comply with the following standards.

1. All outdoor equipment and machinery shall be stored in a neat and organized manner.
2. Outdoor storage of inoperable vehicles and equipment is prohibited.
3. Maintenance and repair of vehicles and equipment may be performed only within a completely enclosed building.
4. Equipment and vehicles shall not be stored with their lifting arms, booms, blades, buckets, scrapers, etc. in a position higher than the main body of the equipment or vehicle.

4.6.2 Live-Work Units

1. **Purpose.** Live-work units are intended to be occupied by business operators (especially artisans and artists) who live in the same structure that contains the commercial or industrial activity. A live-work unit is intended to function predominantly as workspace with incidental residential accommodations being an incidental use.
2. **Applicability and Allowed Uses.** The provisions of this section shall apply to live-work units as allowed by Table 4-2. The nonresidential component of a live-work project shall be only a use allowed within the OTMU, OTCR, and OTIC districts.
3. **Limitations on Use.** A live-work unit shall not be established or used in conjunction with any of the following activities:
 - a. Adult businesses;
 - b. Vehicle maintenance or repair (e.g., body or mechanical work, including on boats and recreational vehicles), vehicle detailing and painting, upholstery, etc.);
 - c. Storage of flammable liquids or hazardous materials beyond those normally associated with a residential use, as subject to review and approval by the Department Director and the Fire Department;

- d. Other activities or uses not compatible with residential activities and/or that could affect the health or safety of live-work unit residents because of dust, glare, heat, noise, noxious gases, odor, smoke, traffic, vibration, other adverse impacts, or hazardous materials, processes, products, or wastes, as determined by the Department Director.

4. **Development Standards.**

- a. Floor Area Requirements. The minimum total floor area of a live-work unit shall be 1,000 square feet. All floor area, other than that reserved for living space, shall be reserved and regularly used for working and display space.
- b. Floor Height Requirements. The minimum floor-to-ceiling height for nonresidential portions of a live-work unit fronting a public street and located at street level shall be 12 feet.
- c. Street Frontage Treatment. Each live-work unit shall have a pedestrian-oriented frontage that publicly displays the interior of the nonresidential areas of the structure. The first 25 feet of the floor area depth at the street-level frontage shall be limited to display and sales activity.
- d. Access to Units. Where more than one live-work unit is proposed within a single structure, each live-work unit shall be separated from other live-work units and other uses in the structure. Access to individual units shall be from common access areas, corridors, or hallways. Access to each unit shall be clearly identified to accommodate emergency services.
- e. Integral Layout. The living space within the live-work unit shall be contiguous with, and an integral part of, the working space with direct access between the two areas, and shall not be a separate stand-alone dwelling unit. The residential component shall not have a separate street address from the business component.

- 5. **Parking.** Each live-work unit shall be provided with at least 2 off-street parking spaces for each 1,000 square feet of floor area. The Department Director may modify this requirement for the use of existing structures with limited parking.

6. **Operating Standards**

- a. Occupancy. A live-work unit may be occupied and used only by a business operator and/or a household of which at least one member is a business operator.
- b. Sale or Rental of Portions of Unit. No portion of a live-work unit may be separately leased, rented, or sold.

Development Regulations

- c. Notice to Occupants. The owner or developer of any structure containing live-work units shall provide written notice to all live-work occupants and users that the surrounding area may be subject to levels of dust, fumes, noise, or other impacts associated with commercial and industrial uses at higher levels than would be expected in more typical residential areas.
 - d. Nonresident Employees. Up to two persons who do not reside in the live-work unit may work in the unit, unless this employment is prohibited or limited by a Use Permit.
7. **Required Findings.** The approval of a live-work unit shall require the Department Director to make all of the following findings:
- a. The establishment of live-work units will not conflict with nor inhibit other uses in the area where the project is proposed;
 - b. The structure containing live-work units, and each live-work unit within the structure, has been designed to ensure that they will function predominantly as work spaces with incidental residential accommodations that meet basic habitability requirements in compliance with applicable regulations; and
 - c. Any changes proposed to the exterior appearance of an existing structure will be compatible with adjacent uses and structures.

4.6.3 Mixed-Use Development

For the purpose of this section, mixed-use projects are developments that combine nonresidential and residential uses or structures on a single parcel, or as components of a single development.

1. **Mix of Uses.** A mixed-use project shall combine residential uses with commercial, office, or limited industrial uses. Mixed-use projects may provide nonresidential uses on the ground floor with residential units above or distributed horizontally on a site.
2. **Residential Density.** The allowable density of a mixed-use project shall be as allowed in the OTMU, OTCR, and OTIC land use districts, as illustrated in Table 4-2, and any bonuses or incentives that are approved by the Town of Yucca Valley.
3. **Residential Uses in OTIC District.**
 - a. Purpose. The purpose of this section is to provide specific guidance for residential and mixed-use development within the OTIC land use district.

- b. Required Findings. No residential or mixed-use within the OTIC land use district may be approved unless and until the Town of Yucca Valley has made all of the following findings:
- (1) The design and placement of residential use demonstrates that the use will not be detrimental or adversely affect the use and function of existing nonresidential uses;
 - (2) The establishment of residential or mixed-use units will not conflict with nor inhibit other uses in or adjacent to the area where the project is proposed;
 - (3) The residential or mixed-use units will not be designed or placed on a parcel in such a way that would expose residential uses to objectionable noise, odor, or other nuisances from existing uses;
 - (4) Site access and circulation for residential uses will not compete with or share access with existing nonresidential uses.
 - (5) Residential or mixed-use units will provide all required parking onsite.

4. **Parking – Mixed-Use Projects**

- a. Residential Uses. A minimum of one parking space per bedroom for each dwelling unit shall be provided, except for senior housing, for which a minimum of one parking space per dwelling unit shall be provided. Parking spaces shall be fully enclosed.

One additional parking space per dwelling unit shall be provided for guests. Guest parking shall be accessible, screened from view of the street, and clearly identified with the words "GUEST PARKING" painted in the space with minimum 8-inch-high letters.

- b. Nonresidential Uses. Off-street parking for nonresidential uses shall be provided for each separate use in compliance with applicable sections of this Specific Plan and the Yucca Valley Municipal Code.

c. Parking and Access Standards

- (1) All parking spaces required for the residential use shall be provided on-site.
- (2) Parking spaces to serve residential uses shall be specifically designated and shall be reserved for the exclusive use of residents.
- (3) If structured parking is provided for the entire complex, separate dedicated and accessible areas shall be provided for residential and commercial uses.

Development Regulations

- (4) Whenever possible, separate site access drives shall be provided for the residential uses and commercial uses.
- (5) Security gates are recommended for access to residential uses and residential parking areas, as well as for commercial parking areas when businesses are closed.
5. **Trash Collection Areas.** Trash collection areas shall be contained within an enclosed structure. Trash collection areas shall be designed, located, or screened so as not to be readily identifiable from adjacent streets.
6. **Loading and Storage Facilities.** Loading areas and solid waste storage facilities shall be located as far as possible from the on-site residential units and shall be completely screened from view from adjacent residential portions of the project. The location and design of the solid waste enclosures shall take into account potential nuisances from odors and noise from collection vehicles.
7. **Private Storage Space.** A minimum 90 cubic feet of private storage space shall be provided for each residential dwelling unit outside the unit, unless a private attached garage, serving only the dwelling unit, is provided. Such storage space shall have a minimum horizontal surface area of 24 square feet and shall be fully enclosed and lockable.
8. **Exterior Lighting.** Parking lot lighting and security lighting for commercial uses in mixed-use projects shall be appropriately shielded so as not to spill over into residential areas. Residential units shall also be shielded from illuminated commercial signs.
9. **Exterior Equipment.** All exterior-mounted equipment shall be screened from view. Special consideration shall be given to the location and screening of noise-generating equipment (e.g., air conditioning, exhaust fans, and refrigeration units). Noise-reducing screens and insulation may be required where equipment has the potential to impact residential uses. Satellite dish systems shall be roof mounted and screened from view.
10. **Outdoor Space for Residential Uses – Mixed-Use Projects**
 - a. A minimum outdoor space of 100 square feet shall be provided for each dwelling unit.
 - b. Outdoor space may be provided as common or private open space. Any common outdoor space shall have a minimum level surface dimension of 20 feet and a minimum area of 400 square feet.
 - c. Outdoor space intended for use by residents only shall not be accessible from the commercial areas.
 - d. Open space and courtyards located in the commercial areas may be accessible to residential occupants and visitors.

e. Landscaping and seating shall be permanently integrated into all required outdoor spaces.

11. **Hours of Operation.** The Town of Yucca Valley may restrict the hours of operation of nonresidential uses to mitigate adverse impacts on the residential uses.

12. **Joint Owners' Association.** For integrated mixed-use projects, a joint tenant/owners' association shall be formed to ensure the well-being of each tenant on-site. The association's bylaws shall specify the following: maintenance and landscaping responsibilities, trash facility responsibilities, parking facility maintenance responsibility, assignment of parking spaces per each use, relationship between uses regarding association representation, voting procedures, and ways to solve problems between the different uses. The association's bylaws shall be subject to review by the Town Attorney.

13. **Building Design**

a. Design Standards. A mixed-use project shall be designed and constructed to:

- (1) Be compatible with and complement adjacent land uses;
- (2) Be compatible with the desert landscape and climate;
- (3) Maintain or enhance the character of development in the immediate neighborhood;
- (4) Maintain or increase the existing number of residential units generally, and specifically those for seniors and a variety of income levels; and
- (5) Mitigate to the maximum extent feasible impacts such as glare, light (including light spill into the night sky), noise, and traffic.
- (6) The architectural style and use of materials shall be consistent throughout the entire project. Differences in materials and/or architectural details shall occur on a structure only where the intent is to differentiate between the residential scale and character of the structure and the commercial scale and character.

b. Separate Entrances. When residential and commercial uses are provided in the same structure, separate entrances shall be provided for each use.

c. Distance Between Dwellings. A minimum distance of 10 feet shall separate exterior walls of separate buildings containing dwelling units on the same lot. The windows or window/doors of any dwelling unit shall not face the windows or window/doors of any other dwelling unit unless separated by a distance of 10 or more feet, except where the

Development Regulations

angle between the walls of the separate dwelling units is 90 degrees or more. Walls parallel to each other shall be considered to be at a zero-degree angle.

- d. Sound Mitigation. Residential units shall be designed to be sound-attenuated against present and future project noise. New projects, additions to existing projects, or new nonresidential uses in existing projects shall provide an acoustical analysis report by an acoustical engineer that describes the acoustical design features of the structure required to satisfy the exterior and interior noise standards of the Yucca Valley Municipal Code.
- e. Rooftop Equipment. Rooftop equipment, except solar energy equipment, shall be completely enclosed on all sides or screened from view from public rights-of-way.
- f. Landscaping
 - (1) All street setback areas and other areas not occupied by buildings, parking, driveways, walkways, and other incidental residential activities shall be fully landscaped with live plant materials and shall be permanently maintained in a neat and orderly manner.
 - (2) A minimum of 10 percent of the total parking lot area shall be permanently landscaped. If a parking area that is within a required setback is landscaped, the landscaped area may be counted toward meeting the minimum landscaping area requirement for the project.
 - (3) For the purposes of this section, permanent landscaping shall consist of landscaped areas at the ground level.
 - (4) Decorative design elements (e.g., fountains, sculptures, planters, rocks, native natural materials, and other similar elements) may be allowed where they are integral parts of a landscape plan composed primarily of live plant materials.
 - (5) Pedestrian walks and vehicular accessways shall be allowed in landscaped areas, but shall not be counted as landscaped areas.
 - (6) The street setback area shall incorporate landscaping and shall not consist of paving only.
 - (7) When appropriate, permanent and automatic irrigation facilities shall be provided in all permanent landscaped areas except potted containers.
 - (8) Landscaping shall be permanently maintained in substantial conformance with an approved landscaping plan.

g. Lighting:

- (1) Lighting design, use, and location shall comply with the Town of Yucca Valley Night Sky Ordinance.
- (2) Light fixtures on uncovered parking areas, vehicle accessways, and walkways shall not exceed a height of 16 feet, except that the maximum height of fixtures on the rooftop of any parking structure on a lot adjacent to any residential zone shall not exceed a height of 8 feet. The overall height shall be measured from the paved surface to the uppermost part of the light standard, including the light globe.
- (3) Lighting shall be directed onto the driveways, walkways and parking areas within the development and away from adjacent properties and public rights-of-way. Appropriate shields shall be incorporated into lighting fixtures to ensure that lighting does not spill onto adjoining properties.

h. Laundry Facilities – Residential Uses:

- (1) Laundry facilities and/or hook-ups shall be provided on-site to serve the residential dwelling units.
- (2) Laundry facilities (i.e., washer and dryer appliances connected to utilities) shall be provided in each individual dwelling unit where there are 3 or fewer dwelling units on a lot. Where there are more than 3 dwelling units on a lot, laundry facilities shall be provided in either each individual dwelling unit or in a common laundry room.
- (3) A common laundry room shall be in an accessible location and shall have at least one washer and one dryer for each 10 dwelling units, maintained in operable condition and accessible to all tenants daily between the hours of 7:00 A.M. and 10:00 P.M.
- (4) Common laundry rooms shall provide lighting and be designed and located in such a way that the interior of the facility can be clearly viewed from the outside. Lighting control switches in common laundry rooms shall not be accessible to the general public.

4.6.4 Outdoor/Sidewalk Dining

1. **Applicability.** Outdoor/sidewalk dining areas that encroach into the public right-of-way shall be subject to obtaining an encroachment permit from the Town of Yucca Valley.
2. **Standards.** Outdoor/sidewalk dining areas shall be subject to the following standards;

Development Regulations

- a. Outdoor/sidewalk dining shall maintain a 4-foot minimum clearance along the public sidewalk, exclusive of landscape areas, fire hydrants, streetlights, other street furniture, and on-street auto overhang.
- b. Applicants requesting an encroachment of the public right-of-way shall submit a brief description of the proposed encroachment and its proposed location in the right-of-way.
- c. Applicants shall submit, to the Town of Yucca Valley, a properly scaled and dimensioned site plan showing existing site conditions, including street and sidewalk width, and the location and dimension of all existing and proposed street furniture and other elements on the sidewalk. Applicants shall identify the number of tables and seating requested and the maximum number of seating in the outdoor area.
- d. An applicant's permit for outdoor/sidewalk dining may be revoked by the Town of Yucca Valley after a 30-day written notice. The Town of Yucca Valley shall provide the applicant with findings of fact for the revocation of the permit.
- e. Sound amplification devices or sound reproduction devices shall not be used or operated with outdoor/sidewalk dining unless approved by a temporary use permit.
- f. Lighting shall comply with the standards set forth in this Specific Plan and applicable sections of the Yucca Valley Municipal Code.
- g. Prior to the approval of any permit, the Department Director shall make a finding that the outdoor/sidewalk dining does not adversely affect the neighborhood or is detrimental to persons working, living, or visiting the area.
- h. A liability insurance policy naming the Town of Yucca Valley as additionally insured for a minimum of \$1,000,000 shall be kept on file with the Town of Yucca Valley.

4.6.5 Nightclubs and Entertainment Uses – Accessory to Restaurant Use Only

1. Nightclubs, bars, and similar establishments shall comply with the requirements set forth in the Yucca Valley Municipal Code.

4.6.6 Parking Structures and Garages

1. **Applicability.** The following standards shall apply to enclosed parking structures, including those above grade and below grade.

2. **Site Organization**

- a. Where appropriate, parking garages shall incorporate a ground-floor retail use adjacent to the public sidewalk.
- b. A minimum 5-foot landscaped setback shall be provided on all sides of the parking structure, except where ground-floor retail space is provided. Landscaping shall include adequate facilities to enable proper maintenance.

3. **Access and Circulation**

- a. Vehicle-stacking areas for entering and exiting traffic shall be of sufficient length to minimize vehicle stacking onto surrounding streets or within the parking structure. A minimum of two vehicle lengths of stacking distance shall be provided between the street and the control gate.
- b. Exit lanes shall be provided at a ratio of one lane for each 200-250 vehicles. The maximum aisle length shall not exceed 400 feet without providing a cross aisle.
- c. Ramp grades shall not exceed ten percent and parking areas shall not exceed a slope of four to five percent.

4. **Lighting and Security**

- a. A minimum illumination of 5 foot-candles shall be provided inside the structure and a minimum of 3 foot-candles for exterior parking areas. Higher levels are recommended for remote areas subject to security considerations (e.g., stairways, elevators, and other pedestrian access points). Minimum illumination levels, measured from the adjacent finished floor, shall be as provided in Table 4-3.

**Table 4-3
Minimum Illumination Levels
For Parking Structures and Garages**

Facility	Minimum Illumination Level
Stairways and exits	5 foot-candles
Interior driving aisles at centerline	5 foot-candles
Interior parking areas at barrier railings	0.5 foot-candle
Roof parking areas	0.5 foot-candle

- b. Lighting fixtures shall be designed and placed to provide uniform illumination over all parking areas.
- c. Light sources shall be shielded so that the source of the illumination is not seen from outside the structure.

Development Regulations

- d. The architectural design of the garage shall eliminate possible hiding places and openings that could allow random pedestrian access.
- e. During periods when parking activity is substantially less than the garage capacity (as at night), there shall be a means of securing unused parking levels from use, including stairwells and elevators. If the garage is not operated on a 24-hour basis, the entire facility shall be secured from access during hours when the facility is closed to normal business activities.
- f. At a minimum, stair towers shall include glass, or appropriate visually penetrable material running vertically along the height of the tower. Elevators shall be provided with glass-backed cabs and shafts.
- g. Stairs and elevators should be located adjacent to a street on the exterior of the structure where lobbies can be exposed to outside view.

5. Building Design

- a. Parking garages shall be designed to help reduce the mass and scale of the garage and to ensure their compatibility with surrounding uses. The following design guidelines shall apply to parking structure design.
- b. Vehicles shall be concealed from view through a combination of screen walls and plantings.
- c. The design of exterior and interior elevations shall provide an adequate level of design detail to reduce a monolithic appearance. This can be accomplished through a combination of the following methods:
 - (1) Minimize horizontal and vertical banding by balancing both horizontal and vertical elements.
 - (2) Incorporate simple, clean geometric forms, and coordinated massing; step back upper levels of the garage.
 - (3) Use ground-floor retail or other uses adjacent to the setback.
 - (4) If the parking garage contains other uses, coordinate openings in the parking garage with the size and modulation of adjacent windows, structural bays, and storefronts.
 - (5) Design openings in the parking garage to resemble architectural elements of the adjoining structure.

- (6) Use masonry materials that are compatible with the natural desert landscape; minimize the use of unpainted concrete.
- (7) Avoid a sloping ramp appearance by providing level and uniform spandrels.
- (8) Visually define and differentiate between pedestrian and vehicular entrances through appropriate architectural detailing.

4.7 Off-Street Parking and Loading Standards

4.7.1 Purpose

The purpose of this section is to ensure that parking and loading areas are sufficient and are properly designed and located. Any use, including a change in or expansion of an existing use or structure, shall have appropriately maintained off-street parking and loading areas in compliance with the standards as described in this Specific Plan. For standards and provisions that are not explicitly stated, the standards and provisions of the Yucca Valley Municipal Code shall apply.

4.7.2 Regulations for Off-Street Parking

Off-street parking and loading for uses within the Old Town Specific Plan area shall be provided in accordance with applicable sections of the Yucca Valley Municipal Code, with the following modifications:

1. **Fractional Spaces.** Fractional spaces shall be rounded upwards.
2. **On-Street Parking.** Existing parking spaces for standard-sized vehicles along a public street that abut a parcel are eligible to meet part or all of the parking requirements for the development on that parcel. For parcels fronting on State Route 62 within the OTMU district, 50 percent of the on-street parking spaces for standard sized vehicles within 100 feet of a parcel or 50 percent of the spaces that will be within 100 feet upon completion of planned street and parking improvements (whichever is greater) may be counted toward the required number of parking spaces for commercial or mixed-uses.
3. **Off-Street Parking Reduction.** For parcels with mixed-use development in the OTMU, OTCR and OTIC districts, the number of off-street parking spaces required by the Yucca Valley Municipal Code may, upon approval by the Department Director, be reduced by the greater of the number of spaces listed below.

If the parcel is within 400 feet of a public parking lot or parking structure, a reduction of 10 spaces or 25 percent of the required number of spaces (whichever is greater) shall be allowed.

Development Regulations

4. **Structure Parking.** A ten percent reduction of required parking spaces for all new development may be approved by the Department Director if 50 percent or more of required off-street parking spaces are located within an on-site parking structure.
5. **Location of Parking.** Required parking spaces for commercial or mixed-uses shall be located on the same parcel or on another parcel not farther than 400 feet from the parcel they are intended to serve. On-site parking areas shall be located as specified in Table 4-4.
6. **Parking Study Required- Shared Parking.** The utilization of shared parking facilities within the Old Town Specific Plan project area is strongly encouraged. Shared parking standards are based on the assumption that patrons will use a single parking space for more than one destination in certain locations within the Specific Plan area and that one parking space will be open and available for short-term parking to serve different uses that may have different peak hours. Shared parking shall be provided in accordance with the following provisions:
 - a. **Eligible Development.** The following categories of development shall be eligible to use shared parking to meet parking requirements:
 - (1) Commercial or mixed-use new construction on sites of less than 30,000 square feet in size;
 - (2) New construction on sites greater than 30,000 square feet in size for retail commercial, restaurants, community centers, museums, and movie theaters; and
 - (3) Additions to existing buildings, rehabilitation of existing buildings, or changes in use or occupancy of existing buildings.
 - b. **Ineligible Development.** The following types of uses are not eligible to use shared parking standards:
 - (1) New or existing residential uses not part of a mixed-use development; and
 - (2) New construction of hotel or office uses on sites greater than 30,000 square feet.
7. **Parking Study Required – Sites Greater Than 30,000 Square Feet.** All commercial uses, and any mixed-use development within the Old Town Specific Plan area that exceeds a site area of 30,000 square feet, shall be required to prepare a parking and loading study, or other study approved by the Department Director, that demonstrates adequate parking for average daily demand (rather than peak-day demand).
 - a. For any development larger than 30,000 square feet, a parking study may be used in lieu of the standards and provisions of this Specific Plan, provided that the following conditions are met:

- (1) The parking study provides sufficient factual justification for modifications to prescribed standards;
 - (2) The project demonstrates the use of creative design concepts, including, but not limited to, mixed-use, shared parking facilities, transit access, pedestrian amenities, and bicycle amenities.
8. **Parking Study Option – Sites of 30,000 Square Feet or Less.** At the discretion of the Department Director, commercial uses and mixed-use development within the Specific Plan area that are 30,000 square feet or smaller shall be provided the option of preparing a parking and loading study that justifies the need for parking and loading facilities, provided the following conditions are met:
- a. The parking study sufficiently justifies modifications to prescribed standards; and
 - b. The project demonstrates the use of creative design concepts, including but not limited to Mixed-Use, shared parking facilities, transit accessibility, pedestrian amenities and bicycle amenities.
9. **On-Street and Common Loading.** As approved by the Department Director, the following loading requirements may apply:
- a. Within the OTMU District, multi-story, mixed-use development that has less than 10,000 square feet of gross leasable area may utilize on-street loading when it is demonstrated that the loading activity will not be detrimental to the public health, safety, or welfare, and will not adversely affect traffic patterns.
 - b. Uses within a mixed-use development may utilize common loading facilities when demonstrated that the loading activity will not be detrimental to the public health, safety, or welfare, and will not adversely affect traffic patterns.

4.8 Landscaping, Walls, and Fences

4.8.1 Landscaping - General

All projects shall provide and maintain landscaping and irrigation in compliance with applicable sections of this Specific Plan and the Yucca Valley Municipal Code. Standards for the provision of landscaping within the public right-of-way in conjunction with any private development shall be in compliance with the Yucca Valley Municipal Code.

4.8.2 Required Preservation of Native Vegetation and Trees

All project applicants shall consult with the Town of Yucca Valley prior to the removal or modification of any existing tree or any native vegetation within private property. It is the intent of

Development Regulations

this section to preserve historically important vegetation and tree resources within the Town of Yucca Valley community.

4.8.3 Landscaping

1. **Landscaping Adjacent to Streets.** All street-adjacent landscaped areas shall include a combination of landscaping materials consistent with a desert climate. For properties that provide no setback, planters, potted plants, and/or other appropriate materials shall be provided.
2. **Required Drought-Tolerant Landscaping.** All landscaping shall be installed and maintained to minimize irrigation demand. All landscape materials, including shrubs, trees, vines, perennials, and ground cover, shall demonstrate drought-tolerant features consistent with the High Desert Water District Conservation Ordinance requirements.

4.8.4 Walls and Fences

1. **Height and Location.** Perimeter fences and walls that are within a front yard setback shall not exceed a height of 3 feet unless specifically allowed (by the Department Director) to be up to 6 feet high to achieve a specific screening purpose.
 - Exception: Walls adjacent to mobile home sites that front the public right-of-way shall be allowed to be up to 6 feet high, subject to the requirements of this section and with approval by the Department Director.
2. **Design.** The design of walls and fences shall be consistent with the overall project design and/or adjacent streetscape. Additionally, the design shall be complementary to the desert landscape.
3. **Walls and Fencing.** Perimeter walls and fences shall be articulated by varying the wall plane at least every 50 feet. The design of the wall shall include an appropriate mix of materials and landscaping.
4. **Chain Link and Fencing.** Chain link or similar fencing shall not be allowed within the public view.

4.9 Signs

Except as otherwise stated in this section, the sign standards and review procedures in Chapter 7, Division 7, Title 8, of the Yucca Valley Municipal Code, as amended, shall apply to all signs within the Old Town Specific Plan project area.

4.9.1 Monument Signs

1. **Allowed Signs.** Monument Signs shall be allowed in the OTCR, OTHC and OTIC districts.
2. **Number of Signs.** One monument sign per property frontage is allowed.
3. **Sign Height.** Monument signs shall have a maximum height of 6 feet.
 - Exception: Monument sign heights in the OTMU and OTHC districts may exceed 6 feet to accommodate desirable architectural features or other elements that contribute to quality design, but shall be subject to approval by the Department Director.
4. **Sign Area and Copy.** A maximum of 40 square feet of sign per face shall be allowed.
5. **Illumination.** Monument signs may be internally illuminated, provided only the sign copy is illuminated; however, external illumination is preferred. External illumination shall illuminate only the monument sign and provide consistency with the architectural design of the primary structure on the site.
6. **Design.** Monument signs shall be consistent with the Old Town Specific Plan Design Guidelines.
7. **Spacing.** There shall be a minimum of 75 feet between monument signs to ensure proper visibility of all signs. The Department Director may waive this requirement in situations where its enactment may be impractical due to the location of signs on adjacent properties.
8. **Location.** Monument signs shall not project over or into public property or easements. Monument signs shall not obstruct traffic sight lines or create any visual obstruction that may create hazards to life, health, or safety.
9. **Required Landscaped Base.** Signs shall be located within a landscaped area, with the base of the sign equal to at least twice the area of one face of the sign. A permanent irrigation system shall be provided and landscaping shall be maintained so as to preclude obstruction of the sign copy.

4.9.2 Freestanding Signs in Public Right-of-Way

1. For the purpose of this Specific Plan, freestanding signs shall be defined as signs that stand directly on the ground and are independent from any building or structure within the public right-of-way or landscaped setback. Freestanding signs in the public right-of-way shall be allowed in the OTMU district in compliance with the following conditions:
 - a. Only one freestanding sign per business is allowed.

Development Regulations



Examples of freestanding signs

- b. A maximum of 2 faces is allowed per sign.
- c. Maximum sign size shall be 3 feet high and 2 feet wide.
- d. No external or internal illumination is allowed.
- e. No highly reflective or fluorescent colors are allowed.
- f. No sign may be permanently affixed to any object, structure, or the ground, including utility poles, light standards, trees, or any merchandise or products displayed outside of building. Freestanding signs may be anchored with removable devices to prevent being blown over by winds, etc.
- g. Signs shall be portable, self-supporting, stable, and weighted or constructed to withstand being overturned by wind or contact.
- h. Signs shall be removed during nonbusiness hours.
- i. Signs shall be located directly in front of the building or business.
- j. Signs shall not be placed in such a way as to interfere with pedestrian or vehicular sight lines or corner clearance zones.
- k. Signs shall not be placed in such a way as to obstruct access to a public sidewalk, public street, parking space, fire door, fire escape, or disabled access, or that will, in any way, obstruct the free passage over any portion of the public right-of-way.
- l. Signs shall not obscure or interfere with a public safety device or official public notice.

- m. Signs shall maintain a minimum of 4 feet for pedestrian clearance over the entire length of the sidewalk or public right-of-way in front of the business.
2. **Placement of Freestanding Signs in the Public Right-of-Way.** In addition to the standards specified in Section 4.9.B above, freestanding signs shall be subject to review and approval by the Department Director. Placement of signs in the public right-of-way shall require the submittal of the following information, and are subject to the following limitations:
- a. Applicant shall submit a brief description of the proposed sign and its proposed location in the public right of way.
 - b. Applicant shall submit an appropriately scaled and dimensioned site plan showing local conditions, including width of the street and sidewalk, and the location and dimension of all street furniture and elements on the sidewalk
 - c. The description and site plan are subject to the review and approval (at his or her sole discretion) of the Department Director.
 - d. The approval of a freestanding sign may be revoked by the Town of Yucca Valley after a 30-day notice. The Town shall make findings of fact for any revocation action.
 - e. A liability insurance policy naming the Town of Yucca Valley as additionally insured in the amount of \$1,000,000 shall be on file with the Town.
3. **Signs for Shopping Centers, Office Complexes, and Mixed-Use Development.** Integrated office complexes, commercial centers, and mixed-use development in the OTMU, OTHC, OTCR, and OTIC districts with a minimum of 200 feet of street frontage shall be subject to the following:
- a. One freestanding identification sign is allowed near the primary street entrance to the center that identifies up to five primary tenants of the center.
 - b. The maximum size is 100 square feet per sign face.
 - c. The maximum height is 10 feet.
 - d. The minimum streetside setback is 10 feet.
 - e. Signs shall be located within a landscaped area equal to at least twice the area of one face of the sign. A permanent irrigation system shall be provided, and landscaping maintained so as to preclude obstruction of the sign copy.

Development Regulations

- f. One additional secondary monument sign is allowed along each street frontage (except for the street on which the primary identification sign is located), in compliance with the following standards:
 - (1) Signs shall not be located directly across from a residential use.
 - (2) No more than three tenants may use one sign.
 - (3) The maximum size per sign face is 50 square feet.
 - (4) The maximum sign height is 5 feet.
 - (5) The minimum streetside setback is 10 feet.

4.9.3 Pole Signs

Pole signs are not allowed within the Specific Plan area.

4.9.4 Awning Signs

Awning signs shall be allowed in all districts, subject to the following requirements.

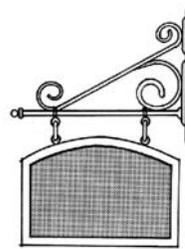
- 1. Awnings with signs shall be located only on structure frontages, including those fronting a parking lot or pedestrian way.
- 2. Signs on awnings are limited to the ground level and second story only.
- 3. A clear distance of at least 8 feet shall be maintained from the lowest part of the awning sign to the ground.
- 4. The maximum area of an awning sign shall be calculated in accordance with the requirements for wall signs in Section 4.9.F.

4.9.5 Projecting Signs

Projecting signs shall be allowed in the OTMU, OTHC, OTIC and OTCR districts, subject to the following requirements:

- 1. Signs shall be located only on the wall frontage with the primary entrance to the structure.
- 2. A clear distance of at least 10 feet shall be maintained from the lowest point of the projecting sign to the ground level. For projecting signs over public driveways, alleys, and thoroughfares, a clear distance of at least 15 feet shall be maintained from the lowest point of the projecting sign to the ground.

3. A sign shall be attached to the wall so that the nearest point of the sign is a maximum of 2 feet from the adjacent wall face.
4. All mounting hardware shall be designed to be compatible with the sign and architectural theme of the building.
5. No part of a sign shall be located within two feet of a curb.
6. Signs may comprise or be configured as logotypes, symbols, or figures in addition to or instead of written words.
7. The maximum area of each sign face shall be 10 square feet.



Example of Projecting Sign

4.9.6 Wall Signs

1. Wall signs shall be located only on walls having frontage along streets, alleys, parking lots, or on-site parking lots, and shall not be located directly across from a residential use.
2. Wall signs shall only consist of individual letters, no panel signs.
3. Wall signs shall not project from the surface from which they are attached more than required for construction purposes and, in no case, more than 6 inches.
4. Wall signs shall not project above the eave of a roof or parapet.
5. Wall signs shall not be mounted in such a way as to obstruct any portion of a window or storefront, except for window signs in compliance with Section 4.9.G, below.
6. For ground-floor storefronts with direct access to the street, alley, or on-site parking lot, wall signs shall have a maximum of 2 square feet of sign area per linear foot of building frontage or tenant space.

Development Regulations

7. One address identification sign for the structure per street frontage is allowed, with a maximum size of 15 square feet.



Example of Wall Sign

4.9.7 Window Signs

Window signs shall be allowed in the OTMU, OTHC, OTCR, and OTIC zoning districts, subject to the following requirements:

1. Signs shall be allowed only on the ground-floor level and second story of a structure frontage.
2. Signs shall not occupy more than 25 percent of the window area.
3. The maximum area for window signs shall be calculated in conjunction with the requirements for wall signs.

4.9.8 Neon Signs

The use of exposed neon signs shall be allowed within the OTMU, OTHC, OTCR, and OTIC districts, subject to the following requirements;

1. Neon signs shall contain no flashing or moving parts.
2. Neon signs and linear tubing shall be Underwriters Laboratory listed, with a maximum of 20 amperes per circuit, and shall be designed to accommodate an automatic dimmer to reduce the brightness of the neon.
3. Neon tubing shall not exceed one-half inch in diameter.
4. Neon lighting shall not be located within 300 feet of a single-family residential property unless the neon lighting is not visible from the residential use. The distance shall be measured in a straight line from nearest point of the proposed sign.

5. Neon tubing shall minimize reflection from any reflective materials in such a manner that it does not obscure the readability of the sign.
6. Neon tubing shall not be used to line storefront windows or doors.
7. Neon tubing shall be allowed to be used as a minor accent for projecting signs or as other building accents, subject to the review and approval of the Department Director.

4.10 Accessory Structures, Equipment, and Utilities

- A. Auxiliary structures, equipment, and utilities shall not be located directly adjacent to the street frontage of any property.
- B. All roof appurtenances, including but not limited to, air conditioning units and mechanical/electrical equipment, shall be shielded and architecturally screened from view from on-site parking areas, adjacent public streets, and adjacent properties. Screening should be designed to be compatible with the architectural design and color of the building.
- C. All ground-mounted mechanical/electrical equipment, including heating and air conditioning units and refuse disposal areas, shall be in a solid enclosed structure and completely screened from surrounding properties by a wall or landscaping, or shall be enclosed within a building. Walls shall be screened with landscaping.
- D. Storage areas shall be enclosed by a solid, architecturally-compatible, masonry wall, with a height adequate to fully screen such areas from public view. Walls shall be screened with landscaping.
- E. Outdoor storage shall not be located adjacent to any street-facing property line, unless storage is the primary use of the site.

4.11 General Operating Standards

4.11.1 Air Quality

1. **Air Pollution.** Sources of air pollution shall comply with rules established by the Environmental Protection Agency (Code of Federal Regulations, Title 40) and the California Air Resources Board. No person shall operate a regulated source of air pollution without a valid operation permit issued by the designated regulatory agency.
2. **Exhaust Emissions.** Construction-related and business activity exhaust emissions shall be minimized by maintaining equipment in good operating condition, in proper tune, and in compliance with manufacturer's specifications. Equipment shall not be left idling for long periods of time.

Development Regulations

3. **Odor Emissions.** Noxious, odorous emissions from a substance or in a volume that is detrimental to, or endangers the public health, safety, comfort, or welfare is a public nuisance, is unlawful, and shall be modified to prevent further emissions release.

4.11.2 Electrical Interference

Activities, processes, and uses shall not operate in a manner that produces electric and/or magnetic fields that adversely affect the public health or safety, or the general welfare of the community, including interference with normal radio, telephone, or television reception from off the premises where an activity is located.

4.11.3 Light and Glare

Lights, spotlights, floodlights, reflectors, and other means of illumination shall be shielded or equipped with special lenses in such a manner as to prevent any glare or direct illumination on any public street or other property.

4.11.4 Noise

Activities, processes, and uses shall not produce noise that may be considered a nuisance or hazard on any adjacent property.

4.11.5 Vibrations

Uses that generate vibrations that may be considered a nuisance or hazard on any adjacent property shall be cushioned or isolated to prevent the generation of vibrations.

4.11.6 Outdoor Storage

Outdoor storage areas for commercial, industrial, and manufacturing uses shall be utilized only for the express purpose of the storage of material or equipment directly related to the use or activity on site. Such outdoor storage shall be fully enclosed by a masonry wall, and the stored material shall be kept below the horizontal plane of the top. Outdoor storage areas shall not be used for manufacturing, assembly, or construction of any equipment or material.

4.11.7 Parking

Parking shall be designed to provide adequate space for access and adequate on-site maneuvering. Loading facilities shall not conflict with or obstruct the proper function of parking facilities.

Off-street parking for one use shall not be considered as providing required off-street parking for any other use, except as expressly authorized by this section.

Parking facilities shall maintain adequate access and maneuverability for emergency vehicles.

4.11.8 Outdoor Activities

Outdoor activities shall be limited to activities that are allowed within the zoning district in which they occur, and shall not limit or obstruct the normal function of adjacent uses. In compliance with the Yucca Valley Municipal Code, temporary outdoor activities shall require a Temporary Use Permit.

4.12 Development Incentives

The following development incentives provide specific guidance for the granting of additional development potential, provided that certain conditions are met. The development incentives in this section encourage the effective utilization and consolidation of parcels to establish more viable development opportunities. The following provisions apply to all land use districts within the Old Town Specific Plan.

4.12.1 Lot Consolidation Incentives

1. **Purpose.** This section provides incentives for the consolidation of adjacent parcels within any land use district of the Old Town Specific Plan. The Town of Yucca Valley encourages the consolidation of real property within the Old Town Specific Plan area as a means to maximize development and redevelopment potential that is consistent with the purpose and intent of this Specific Plan.
2. **Incentives.** The following incentives shall be made available to applicants who consolidate two or more parcels within the Old Town Specific Plan project area;
 - a. Development Intensity (FAR) Bonus. Increased floor-to-area ratio (FAR) bonus of the amount listed in Table 4-4 shall be granted to properties initiating a lot consolidation. The bonus incentives shall apply to the gross square footage of a single parcel following consolidation.

Table 4-4
Lot Consolidation FAR Bonus

Land Use District	Base Intensity (FAR)	Allowable Intensity Bonus
OTMU	1.0	15%
OTHC	0.35	10%
OTIC	0.40	5%
OTCR	0.40	10%

- b. Residential Density Bonus. Residential density bonus incentives shall be provided for the amount listed in Table 4-5. The density bonus incentives shall apply to the gross square footage of a single parcel following consolidation.

Table 4-5
Lot Consolidation Residential Density Bonus

Land Use District	Base Density (Dwelling Units per Acre)	Allowable Density Bonus (Dwelling Units per Acre)
OTMU	40	10
OTHC	—	—
OTIC	30	5
OTCR	24	5

- c. Financial Incentives. At the discretion of the Town of Yucca Valley Redevelopment Agency, and contingent upon available funding as determined by the Agency Board, fee assistance and other financial incentives may be made available to encourage lot consolidation.

(1) *Types of Incentives*

- Financial incentives may include, but are not limited to:
 - Permit fee assistance (waivers, reduced fees, etc.)
 - Shortening the approval procedure timeline.
 - Other incentives, as determined by the Redevelopment Agency Board.

- (2) *Written Requests*. Prior to the approval of any financial incentives, interested parties shall submit a written request for fee assistance to the Department Director. These requests shall be reviewed on a case-by-case basis and granting such requests is contingent upon funding availability, as prescribed by the Town Council.

- (3) *Pre-Application Hearing*. Prior to the approval of any financial incentives, a pre-application meeting with the project applicant(s) and the Department Director is highly recommended. Fees for pre-application meetings shall be waived.

- (4) *Determination*. Prior to the issuance of any permit, the Department Director shall determine the availability and extent of financial assistance. The terms and conditions of the approved financial incentives shall not be modified, extended, or revised.

4.12.2 Façade Improvement Incentives

1. **Purpose.** This section provides incentives to encourage improvement of building façades by offering design assistance or financial incentives established by the Yucca Valley Town Council. These incentives are intended to fulfill the following objectives:
 - a. Provide a financial incentive for property owners within the Old Town Specific Plan project area to upgrade the exteriors of their buildings;
 - b. Promote the retention and attraction of businesses to strengthen the business potential in the Specific Plan area;
 - c. Increase the utilization and restore the economic vitality of buildings within the Specific Plan area; and
 - d. Maintain and enhance the property values and economic benefits of property ownership within the Specific Plan area.
2. **Eligible Improvements.** Improvements may include sign renovation or replacement, wall repairs and repainting, window replacement or modification, door replacement or modification, planter box installation, landscaping, disabled-access improvements, ornamental or decorative features, exterior lighting, awnings, and other improvements that increase the visual quality of the building. Professional services and Town permit fees are also considered eligible expenses.
3. **Nonqualifying Improvements.** Interior improvements are not eligible for façade improvement incentives.
4. **Evaluation Criteria.** The following evaluation criteria shall be used when evaluating any application for Façade improvements:
 - a. Demonstrated upgrade of building façade conditions;
 - b. Maintenance or resuscitation of historic character;
 - c. Enhancement of the pedestrian environment;
 - d. Renovation of prominent locations;
 - e. Consistency with the Old Town Specific Plan Design Guidelines;
 - f. Improvements that will lead to the occupancy of vacant buildings; and

Development Regulations

- g. Facilitation of the retention and growth of existing businesses and expansion of economic activity.
5. **Streamlined Approval.** All eligible façade improvements shall be subject to approval at the staff level. Staff will evaluate each proposal for consistency with the evaluation criteria as described in this chapter.
 - Exception: At the discretion of the Department Director, eligible improvements may require additional design review, subject to the requirements of this chapter.

4.12.3 Mixed-Use Development Incentives

1. **Purpose.** This section provides incentives to encourage mixed-use development within applicable zoning districts of the Old Town Specific Plan. Mixed-use development shall meet the requirements of applicable sections of this Specific Plan and the Yucca Valley Municipal Code. The Town of Yucca Valley encourages the development of mixed-use projects within the OTMU, OTCR, and OTIC land use districts to maximize development and redevelopment potential that is consistent with the purpose and intent of this Specific Plan.
2. **Mixed-Use Requirement.** Mixed-use development incentives shall apply to projects that combine residential and nonresidential units. Any development excluding residential use shall not be considered mixed-use for the purpose of this section.
3. **Development Intensity (FAR) Incentives.** A bonus of increased floor-to-area ratio (FAR) for the amount listed in Table 4-6 shall be granted to project applicants requesting the development of Mixed-Use projects. The bonus incentives shall apply to the nonresidential gross square footage of the development site.

Table 4-6
Mixed-Use FAR Incentives

Land Use District	Base Intensity (FAR)	Allowable Bonus
OTMU	1.0	15%
OTHC	0.35	—
OTIC	0.40	10%
OTCR	0.40	10%

4. **Development Density Incentives.** A development density bonus for the amount listed in Table 4-7 shall be granted to properties requesting the development of mixed-use projects. The bonus incentives shall apply to the residential gross square footage of a single parcel.

Table 4-7
Mixed-Use Residential Density Incentives

Land Use District	Base Density (Dwellings Units per Acre)	Allowable Bonus (Dwellings Units per Acre)
OTMU	40	10
OTHC	—	—
OTIC	30	5
OTCR	24	10

5. **Height Bonus.** A height bonus for the amount listed in Table 4-8 shall be granted to properties requesting the development of Mixed-Use projects. The bonus incentives shall apply to any combination of nonresidential or residential gross square footage of a single parcel.

Table 4-8
Mixed-Use Development Height Bonus

Land Use District	Allowable Height (Stories)	Height Bonus (Stories)
OTMU	3	1
OTCR	3	1
OTIC	3	1

6. **Additional Incentives.** At the discretion of the Town of Yucca Valley, additional intensity or density bonuses may be granted if the project exhibits any one or more of the following:
- a. Exemplary architectural design;

Development Regulations

- b. Further promotion of the purpose and intent of this Specific Plan in terms of development quality, design, and economic opportunity; or
- c. Contribution to increased employment opportunities.

Granting of additional incentives in addition to those allowed in this section shall be subject to review by, and approval of, the Planning Commission.

4.13 Development Review Procedures

4.13.1 Applicability

The procedures and regulatory provisions necessary to administer development review for applicable properties, structures, and uses within the Specific Plan area shall be subject to the requirements of the Yucca Valley Municipal Code, in addition to the provisions in this section.

4.13.2 Uses Allowed by Right

For uses designated as “Allowed” in Table 4-1 of this chapter, project applicants shall be subject to the development review procedures provided in the Town of Yucca Valley Development Code. All applicants are subject to Plan Review, and to miscellaneous permits as determined by the particular use.

4.13.3 Site Plan Review

Site Plan Review permits are intended to streamline review of development proposals that conform to the regulations and standards as set forth in this Specific Plan, but that may have characteristics that necessitate a higher level of discretionary review. Site Plan Reviews are intended to be approved at the staff level.

4.13.4 Conditional Use Permit

1. **Applicability.** An application for a Conditional Use Permit within the boundaries of the Old Town Specific Plan area shall be considered by the Planning Commission and Town Council in compliance with applicable sections of this chapter and the Yucca Valley Municipal Code.
2. **Uses Requiring a Conditional Use Permit.** Conditional Use Permits may be granted for only the following land uses or activities (in addition to those listed in Table 4-1):
 - a. Denied applications appealed by the applicant;
 - b. Mixed-Used development greater than 30,000 square feet;
 - c. Applications for heights greater than 3 stories; and

d. Uses within the Highway Environs Overlay district, subject to the requirements of Section 4.14 of this Specific Plan.

3. **Application Filing, Processing and Review**

a. Exercising Approved Applications. Approved applications shall be exercised within one year of approval. Two one-year time extensions are allowed, subject to separate applications and fees, after which time a new application shall be filed with the Department Director.

b. Run with the land. A Conditional Use Permit that is valid and in effect, and was granted in compliance with the provisions of this Specific Plan, shall run with the land and continue to be valid upon the change of ownership of the land, or of any lawfully existing structure on the land. Conditional Use Permits shall run with the land as long as there is continuous use of the approved use. Any unauthorized change in use may subject the Conditional Use Permit to revocation.

4.14 Highway Environs Overlay District

4.14.1 Purpose

The purpose of the Highway Environs Overlay District is to provide for discretionary review of development proposals in areas where changes in land use regulations are contemplated because of the potential future realignment of State Route 62. The provisions of this section apply in addition to the provisions for each land use district of this Specific Plan.

4.14.2 Specific Plan Land Use Map Designator

The Highway Environs Overlay District may be combined with any base district in the Specific Plan area. The parcels applicable to the Highway Environs Overlay District shall be shown on the Specific Plan land use map.

4.14.3 Review Procedures in the HE Overlay District

1. **Required Site Plan Review.** For all development applications within the Highway Environs Overlay District, Site Plan Review shall be required for the establishment of any new or expanded use or structure.

a. Site Plan Review shall be in compliance with Title 8, Division 3, Chapter 3, Article 12, of the Yucca Valley Municipal Code.

2. **Pre-Application Meeting.** Prior to the submittal of a formal Site Plan Review Permit, a meeting with the Department Director or designee is strongly recommended to discuss the project in

Development Regulations

detail. The purpose of this pre-application meeting is to evaluate the proposed use in consideration of the potential future realignment of Route 62.

4.14.4 Site Plan Review Procedures – Old Town Specific Plan

Site Plan Review Procedures for the Old Town Specific Plan Highway Environs Overlay shall consist of the following:

1. Tier I Site Plan Review – Staff Level Review

Projects requiring minor site review as determined following the Site Plan Review Permit application shall be reviewed at the staff level. The Community Development Director may impose conditions of approval for the approval of the Site Plan Review Permit.

At the discretion of the Department Director, Tier 1 Site Plan Review may be referred to the Development Review Committee or the Planning Commission.

2. Tier II Site Plan Review – Development Review Committee

At the discretion of the Department Director, Site Plan Review may be referred to the Development Review Committee, as determined following the submittal of a Site Plan Review Permit application.

In its Tier II Site Plan Review, the Development Review Committee may impose conditions of approval.

The following activities shall require Tier II Site Plan Review:

- Mixed-use projects of 30,000 square feet or less
- Projects appealed after Tier I review

Site Plan Review Permits denied during Tier II review may be appealed to the Planning Commission at the discretion of the applicant.

3. Tier III Site Plan Review – Planning Commission Review

At the discretion of the Department Director or the Development Review Committee, Tier I and Tier II projects may be referred to the Planning Commission. The following activities shall require Tier III Site Plan Review:

- Mixed-use projects greater than 30,000 square feet;
- Projects requesting density/intensity bonus;
- Staff-referred projects; and
- Projects appealed from the Development Review Committee.

4.14.5 Development Regulations

Development regulations and additional conditions of approval for the Highway Environs Overlay District shall be specified by a Site Plan Review Permit or shall be those of the base district with which the HE Overlay district is combined.

4.14.6 Expiration of HE Overlay District and Renewal

The adoption of the Old Town Specific Plan shall establish the Highway Environs Overlay District. Upon the determination of the future alignment of State Route 62 and acquisition of associated rights-of-way, the provisions of the HE Overlay District shall be terminated.

4.14.7 Re-submittal of Development Proposals

Notwithstanding the provisions of this section, a permit application that has been denied, or approved subject to conditions unacceptable to the applicant, may be resubmitted on or after the effective expiration date of the Highway Environs Overlay District.

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Chapter

5

DESIGN GUIDELINES

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CHAPTER 5: DESIGN GUIDELINES

5.1 Introduction

5.1.1 Purpose of the Design Guidelines

The Old Town Yucca Valley Design Guidelines are intended to promote high-quality design and to ensure that new development and rehabilitation promote a clear community identity and sense of place. It is not the intent of the guidelines to eliminate design freedom, discourage innovative design, or impose an overriding style, particular color palette, or artificial theme. The design guidelines encourage preservation of the Town's historic core, yet allow creativity in new infill development that is compatible with the character of the area. The design guidelines establish the Town's design philosophy for Old Town and are one of the main elements used to evaluate development projects subject to discretionary review.

Discretionary design review carefully examines the quality of a project's site design, architecture, landscaping, and other important features such as signage and lighting. The purpose is to ensure that plans for new development or rehabilitation carefully consider the community context and make a conscious effort to develop a compatible relationship with neighboring properties.

5.1.2 Application of the Design Guidelines

The Old Town Design Guidelines will be applied as part of the City's review of proposed projects through the Town's design review process or through the review of discretionary land use permits. The design elements of each project (including site design, architecture, landscaping, lighting, and signage) will be reviewed comprehensively by the applicable review authority.

As written, the Design Guidelines apply to all the Old Town land use subdistricts described in this Chapter 5 – Old Town Commercial/Residential, Old Town Highway Commercial, Old Town Mixed Use, and Old Town Industrial. Where a particular guideline or set of guidelines is most applicable to a certain area or project type, it is noted in the text.

During the review process, the review authority may interpret these design guidelines with some flexibility in their application to specific projects, as not all design criteria may be workable or appropriate for each project. The overall objective is to ensure that the intent and spirit of the design guidelines are followed.

Design Guidelines

5.1.3 Using the Design Guidelines

The Old Town Design Guidelines are designed to meet the needs of many users, including: property owners, merchants, real estate interests, architects, designers, building contractors, vendors, craftspeople, Town of Yucca Valley, and other interested organizations and persons in the community. Each of these interests has a vital role to play in establishing quality development in Old Town.

When preparing a new project in the Old Town area, or proposing changes to an existing building or site, users of the Old Town Design Guidelines should first consult the map to determine the character area within which the project falls. Secondly, the user should identify any specific design guidelines that are applicable, paying closer attention to guidelines that may be unique to that character area. Lastly, if questions still exist about a particular design issue, the user should refer to the 2001 *Town of Yucca Valley Design Guidelines* for additional guidance.

5.1.4 Organization of Design Guidelines

Each of the remaining sections of this Chapter 5 focuses on a particular design element and its subelements, as follows:

Section 5.2 Site Design

- ◆ Edge Conditions (5.2.1)
- ◆ Internal Site Organization (5.2.2)
- ◆ Relationship to Adjacent Sites and Features (5.2.3)
- ◆ Parking Lot Design and Configuration (5.2.4)

Section 5.3 Architecture

- ◆ Architectural Styles (5.3.1)
- ◆ Building Materials and Color (5.3.2)
- ◆ Building Mass, Form, and Scale (5.3.3)
- ◆ Storefront Design (5.3.4)

Section 5.4 Development Details

- ◆ Landscaping (5.4.1)
- ◆ Public Space and Site Amenities (5.4.2)
- ◆ Lighting (5.4.3)
- ◆ Building Equipment and Services (5.4.4)

Section 5.5 Commercial Signs

- ◆ Sign Materials and Colors (5.5.1)
- ◆ Encouraged Sign Types (5.5.2)

5.2 Site Design

As described in Subsections 5.2.1 through 5.2.4, the important considerations for appropriate site design in any area. These include:

- ◆ The relationship of the project to adjacent public rights-of-way (edge conditions);
- ◆ The internal relationship of the project's key elements, including buildings, parking, site access, circulation, public spaces, and landscaping;
- ◆ The design and function of parking areas; and
- ◆ The relationship of the project to the neighboring property developments.

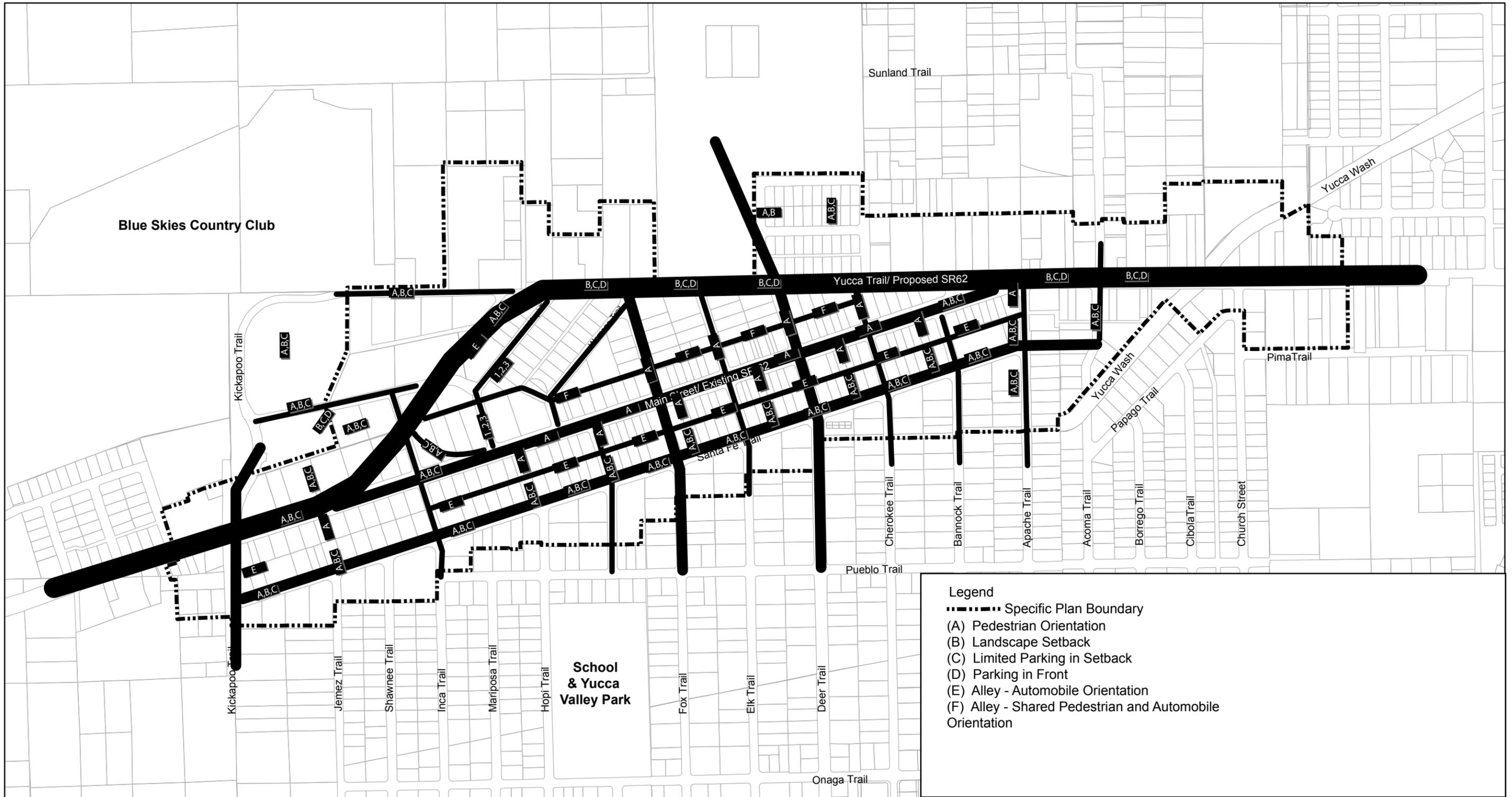
5.2.1 Edge Conditions

The "edge condition" of a particular development (existing or proposed) is defined by the relationship of the building(s) and parking area to the street. In Old Town Yucca Valley, essentially six edge conditions are appropriate for new development. The development standards in Chapter 4 provide the minimum setback requirements for buildings and parking areas, while the following edge conditions determine the appropriate locations of buildings, parking, and frontyard landscaping.

1. Street-Adjacent Buildings with Pedestrian Orientation
2. Semi-Street-Adjacent Buildings with Landscaped Setback
3. Buildings Set Back from Street with Limited Parking in Front
4. Buildings Set Back from Street with Parking in Front
5. Alley with Automobile Orientation
6. Alley with Shared Pedestrian and Automobile Orientation

The edge conditions that apply to each street segment are identified in Exhibit 5-1, *Edge Conditions*.

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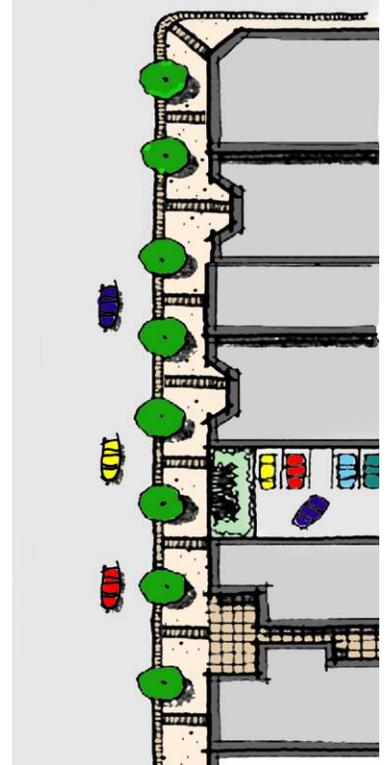


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A. Street Adjacent Buildings with Pedestrian Orientation

Buildings should be built to the front and side property lines to form a continuous line of active building fronts (a “street wall”) along the street, avoiding gaps, and parking should be in the rear.

- ◆ Provide corner cut-offs for buildings on prominent intersections.
- ◆ Place the ground-level front elevation of the building at the sidewalk edge to maintain the continuity of the street wall.
- ◆ Driveway cuts are discouraged along the street edge.
- ◆ Avoid setbacks from the sidewalk edge; where small setbacks occur, use the space for pedestrian plazas.
- ◆ Avoid parking lots that interrupt commercial space; where parking lots exist, create a landscaped buffer along the sidewalk edge.
- ◆ Avoid blank walls and other dead spaces at the ground level; entrances should generally be spaced no more than 25 to 30 feet apart.
- ◆ Create pedestrian paseos into parking lots at the rear of buildings.



Sidewalk-adjacent buildings create a pedestrian-friendly environment.

Design Guidelines

B. Semi-Street-Adjacent Buildings with Landscaped Setback

Buildings should be set back from the front and side property line at varying distances. The setbacks are landscaped, and parking should be in the rear or on the sides.

- ◆ Fully landscape setback areas and interrupt the landscape only with pedestrian areas and sidewalks.
- ◆ Buildings may be clustered, allowing varying setbacks and orientations.
- ◆ Parking should not be located in the front yard setback; parking at the rear of the building is encouraged.
- ◆ Parking lots, if located at the sides of buildings or elsewhere on the site where they may be visible from the street, should be screened from street view by low walls and/or shrubs.
- ◆ Keep driveways to a minimum number and width, within safety requirements.
- ◆ Provide vehicular access to adjoining parcels wherever possible.

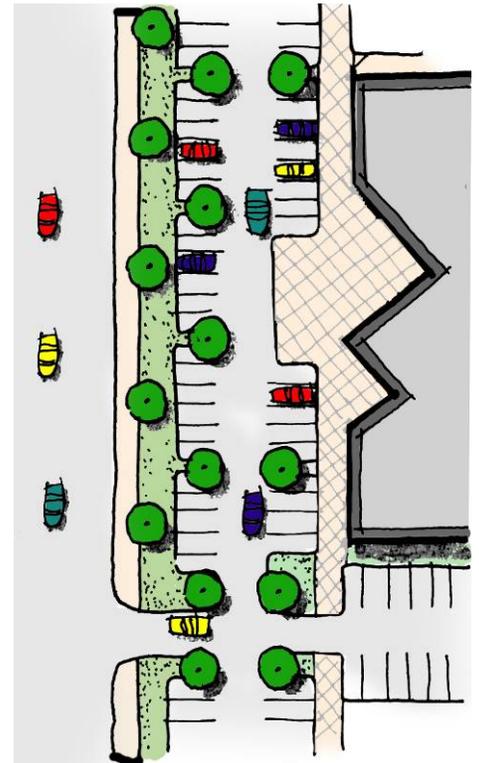


Landscaped setbacks create a soft edge.

C. Buildings Set Back from Street with Limited Parking in Front

Buildings should be set back from the property at a distance to allow for a landscaped pedestrian walkway and (at most) two rows of parking in front of the building; locate additional parking in the rear or on the sides.

- ◆ Only one bay of parking (two rows plus aisle) is encouraged.
- ◆ Provide landscaped pedestrian areas at the primary building entrances.
- ◆ Trees, shrubs, and/or low walls are encouraged to screen the parking area from street view.
- ◆ Accent landscaping and enhanced paving are encouraged to highlight project entries.
- ◆ Provide vehicular access and pedestrian connections to adjoining parcels wherever possible.
- ◆ Parking areas located directly off the street without defined edges are discouraged.



Limited parking in front setback may be appropriate in less pedestrian-oriented areas, such as strip commercial centers or offices.

Design Guidelines

D. Buildings Set Back from Street with Parking in Front

Buildings should be set back from the property at a distance that allows the primary parking to be in the front setback.

- ◆ Set buildings back from the street to allow parking between the street and the building(s).
- ◆ Locating smaller buildings closer to the street edge is encouraged.
- ◆ Provide fully landscaped buffers at the street edge of parking areas; trees, shrubs, and/or low walls are encouraged to screen parking areas from street view.
- ◆ Landscaped pedestrian areas are encouraged at primary building entrances.
- ◆ Provide access to adjoining parcels, wherever possible.
- ◆ Arrange parking bays so that pedestrians walk parallel to drive aisles.

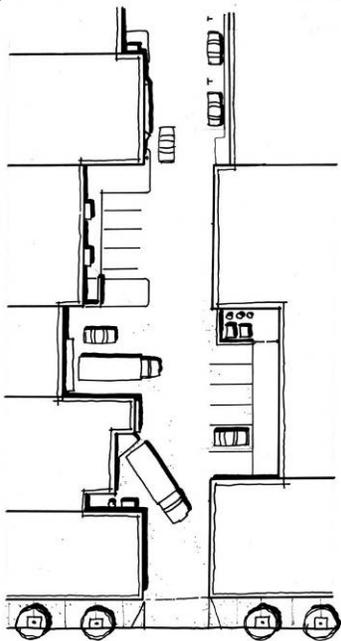


Large parking areas in front setbacks are most appropriate for larger developments and structures, such as shopping centers and industrial buildings.

E. **Alley with Automobile Orientation**

Alleys that are oriented primarily toward vehicular traffic should accommodate parking, circulation, and deliveries.

- ◆ Vary the building setbacks on alley edges.
- ◆ Use setbacks for parking, deliveries, storage, and refuse.
- ◆ Locate alley parking primarily to accommodate employee and delivery vehicles.
- ◆ Improvements to rear facades are encouraged.
- ◆ Screen all refuse containers.



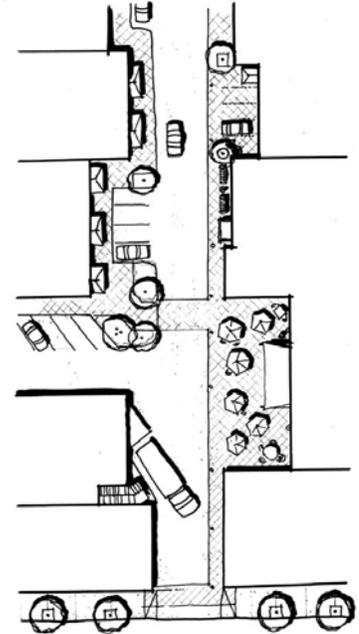
Vehicles are the primary visitors to these alleys.

Design Guidelines

F. Alley with Shared Pedestrian and Automobile Orientation

Alleys oriented for both pedestrian and automobile traffic should include a variety of pedestrian enhancements.

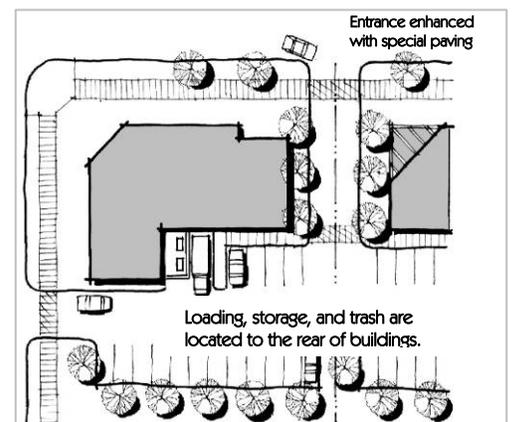
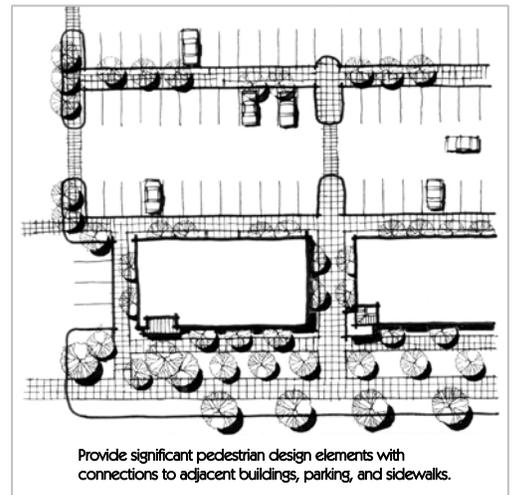
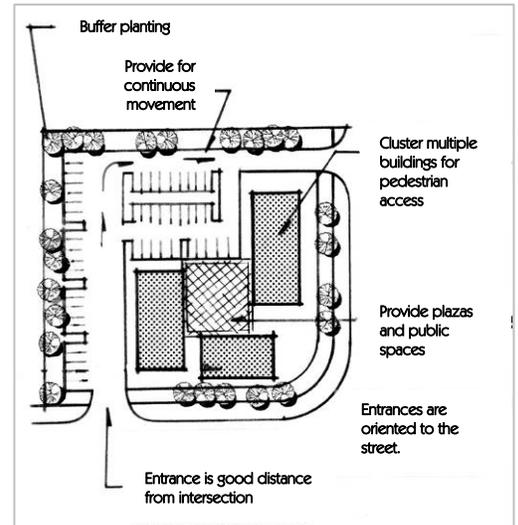
- ◆ Vary the building setbacks on alley edges.
- ◆ Setbacks should accommodate outdoor dining and small plazas, while still supporting parking, deliveries, and refuse containers; screen all refuse containers well.
- ◆ Landscaping, pedestrian-level lighting, special paving, shade structures, benches, and other amenities are encouraged.
- ◆ Rear entrances to businesses along the alley with improved rear facades (e.g., lighting and awnings) are encouraged.
- ◆ Delineated pedestrian walkways are encouraged through the use of special paving and/or landscaping.
- ◆ Pedestrian connections between buildings that create a direct link to the sidewalk are encouraged.
- ◆ Locate alley parking primarily to accommodate visitors and shoppers; clearly identify parking spaces through signage and striping.



Pedestrian-oriented alleys include amenities and details such as special paving, landscaping, and outdoor dining.

5.2.2 Internal Site Organization

- ◆ Locate buildings and on-site circulation systems to minimize conflicts between pedestrian and vehicle; wherever possible, link buildings to the public sidewalk with textured paving, landscaping, etc.
- ◆ For larger projects with multiple buildings, consider clustering buildings around areas of usable open space to provide opportunities for plazas and prevent long “barracks-like” rows of buildings; when clustering is impractical, establish visual links between buildings.
- ◆ When residential and commercial uses are provided in the same structure (mixed-use), provide separate pedestrian entrances and parking facilities for each use.
- ◆ Orient primary entrances toward major streets and sidewalks to encourage a high level of pedestrian activity; provide clearly defined pedestrian paths to primary entrances.
- ◆ Recognize the importance of spaces between buildings as “outdoor rooms” or “paseos” on the site; outdoor spaces should have clear shapes that reflect careful planning and are not simply “left-over” areas between buildings. (See Section 5.4).
- ◆ Organize landscaped areas to define outdoor spaces and to buffer the visual impact of buildings and parking lots. (See Section 5.4).
- ◆ Carefully consider the location of all property line walls and fences; show a detail of the property line wall and how it corresponds with existing, adjacent building walls on site plans and drawings, as well as all roof overhangs. (See Section 5.4).



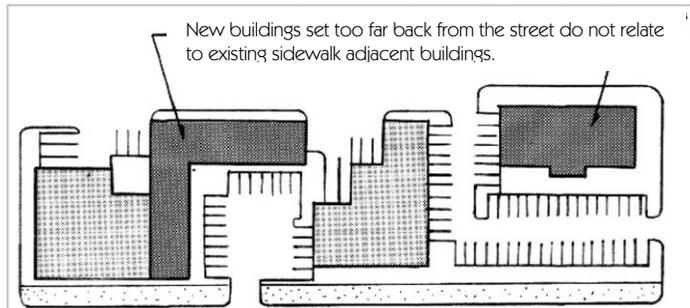
Design Guidelines

- ◆ Locate loading facilities, equipment and service areas, and refuse enclosures at the rear of buildings or in areas where they will be the least visible; screen all such facilities from public view by a combination of dense landscaping, walls, and/or berms. (See Section 5.4).

5.2.3 Relationship to Adjacent Sites and Features

- ◆ Site design should consider the arrangement of buildings, parking, circulation, and landscaping on adjacent sites; locate buildings and open spaces for the mutual advantages of access, circulation, open space, sunlight, and preservation of views.
- ◆ Wherever possible, connect adjacent projects to promote ease of access by pedestrians and automobiles and to reduce traffic movements on adjacent streets; in some cases, only a sidewalk connection may be necessary, while in other cases, shared access and parking will be appropriate.
- ◆ When a new project is developed adjacent to a vacant site, consider how future connections will be accommodated.

- ◆ Show buildings, parking, and other important features on adjacent sites on site plans; consider the site arrangement of adjacent properties and try to create a design that respects the important influences around it.



- ◆ Buffer commercial uses from residential uses as much as possible; use building orientation, landscaping, setbacks, and low walls to provide adequate separation.

- ◆ Where service and work bays are permitted on site, orient them so that the interiors are not visible or audible from adjacent public streets, residential structures, or active open space.

- ◆ Make Yucca Valley's natural environment one of the most important considerations in site design; new development should be sensitive to local natural and built features. Scenic views, climate, and natural amenities unique to the site (such as Joshua Trees and scenic views) should be preserved and incorporated into projects.

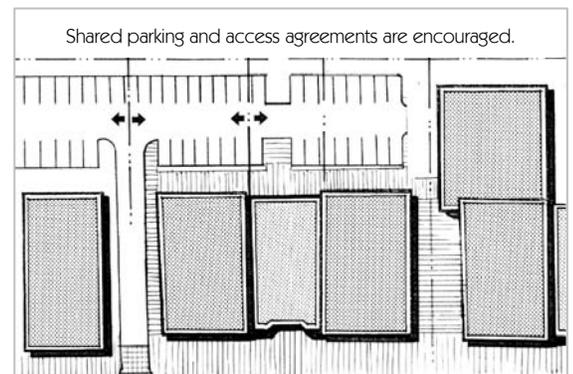
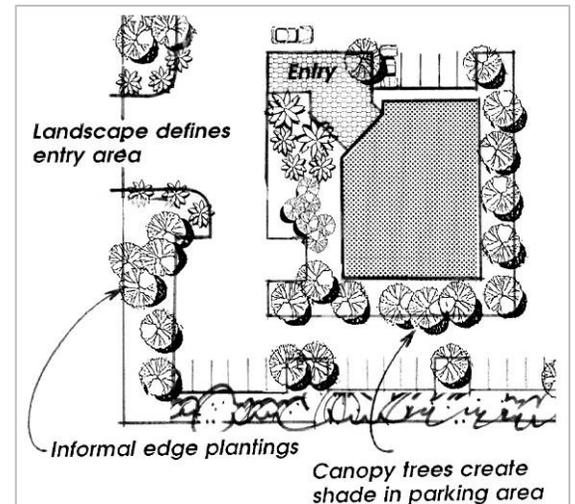


View preservation is an important consideration in site design.

5.2.4 Parking Lot Design and Configuration

Private parking in Old Town should be located according to the appropriate edge condition, as described in the following guidelines.

- ◆ Make building and driveway approach (not the parking lot) the primary visual presence along the major street.
- ◆ If a parking lot is adjacent to a public street, landscape it to screen the visual impact of parked vehicles from the public right-of-way, using planting at the setback line.
- ◆ Separate parking from the buildings with landscaping to reduce the visual impact of paved surfaces, and to provide shade.
- ◆ Minimize the number of access driveways to the site and locate them as far as possible from street intersections; parking lot access points should not interfere with the function of adjacent roadways and should minimize potential conflicts with pedestrians.
- ◆ Common shared-access driveways and shared parking circulation aisles are strongly encouraged in adjacent parking areas.
- ◆ Provide safe, convenient, pedestrian links between parking areas and businesses; link parking areas directly to public sidewalks, pedestrian walkways, mid-block paths, alleys, or open space areas.
- ◆ Ensure visibility for vehicles entering and exiting the site by providing unobstructed sight lines at corners and mid-blocks.
- ◆ Position on-site pick-up and drop-off areas adjacent and parallel to streets and/or drive aisles to allow vehicles to easily exit the main flow of traffic, and to stop. Drop-offs include bus stops, loading areas, and pedestrian drop-off areas.



5.3 Architecture

The construction of new buildings and rehabilitation of existing structures are critical to the continued economic vitality of Yucca Valley. These new and rehabilitated buildings should be compatible with the character of the area in which they are located. New structures should be sympathetic and compatible with the surrounding buildings in terms of mass, scale, height, facade rhythm, placement of doors and windows, and color and use of materials without feeling that they have to precisely duplicate an architectural style from the past to be successful.

5.3.1 Architectural Styles

The following general principles should be considered in determining an architectural style.

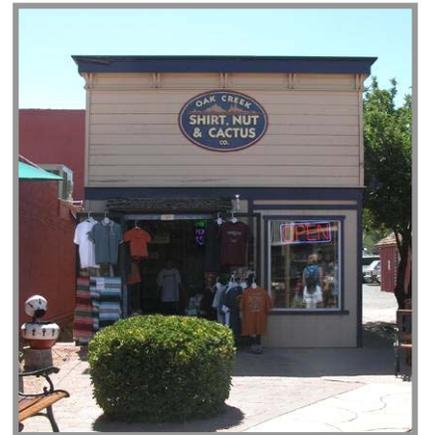
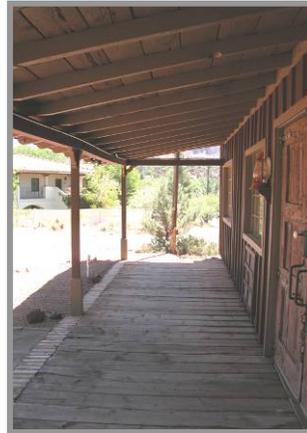
- ◆ Designers should familiarize themselves with the design elements and details used on older buildings in the Yucca Valley area.
- ◆ For new infill buildings, select material carefully and use correct architectural detailing to avoid looking like cheap historic imitations; avoid architectural “gimmicks.”
- ◆ Avoid sharp contrasts between architectural styles on adjacent parcels; do not use dramatic visual contrasts from neighboring structures as attention-getters.
- ◆ For side and rear building facades, provide a level of design detail and finish compatible with that of the front facade, particularly if they are visible from streets, alleys, adjacent parking areas, or residential uses.
- ◆ Franchise architecture with corporate signatures is strongly discouraged.
- ◆ Use special architectural features to accent buildings at prominent street corners and at the terminus of a street corridor or pedestrian way.

While a predetermined architectural theme is not required of new and rehabilitated structures, the following architectural styles and forms are most appropriate in Old Town, as described in the following sections.

1. Territorial/Western
2. Spanish Eclectic/Mission Revival
3. Streamlined Moderne/Art Deco
4. Artisan Industrial
5. Contemporary

1. Territorial/Western

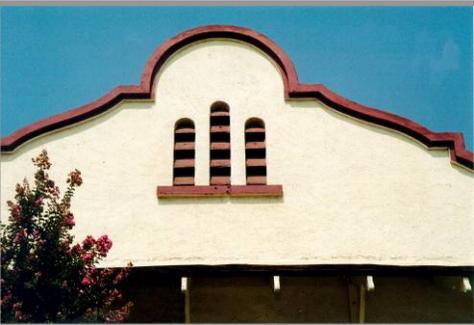
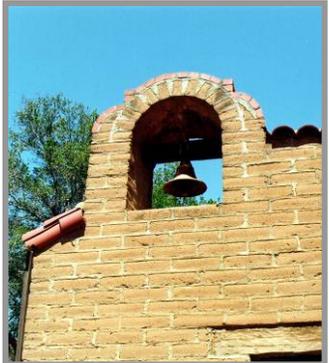
This style is typified by character-defining elements such as flat or stepped-parapet rooflines and covered walkways, natural wood, board and batten, wood shingles, and heavy timber. This style is primarily recommended in the Old Town Mixed Use Subdistrict, but is appropriate in all the land use subdistricts.



Design Guidelines

2. Spanish Eclectic/Mission Revival/Moorish/Desert Architectures of the World

This style is typified by character-defining elements such as varied rooflines and smooth facades made of stucco, reminiscent of adobe structures; red clay tiles; and wide overhanging eaves. This style also draws from an array of historical Spanish architectural details, such as prominent archways, mission-shaped dormers and parapets, second-story balconies (Monterey influence), and heavy timber. This style is most encouraged in the Old Town Commercial/Residential and Highway Commercial Subdistricts.



3. Streamlined Moderne/Art Deco

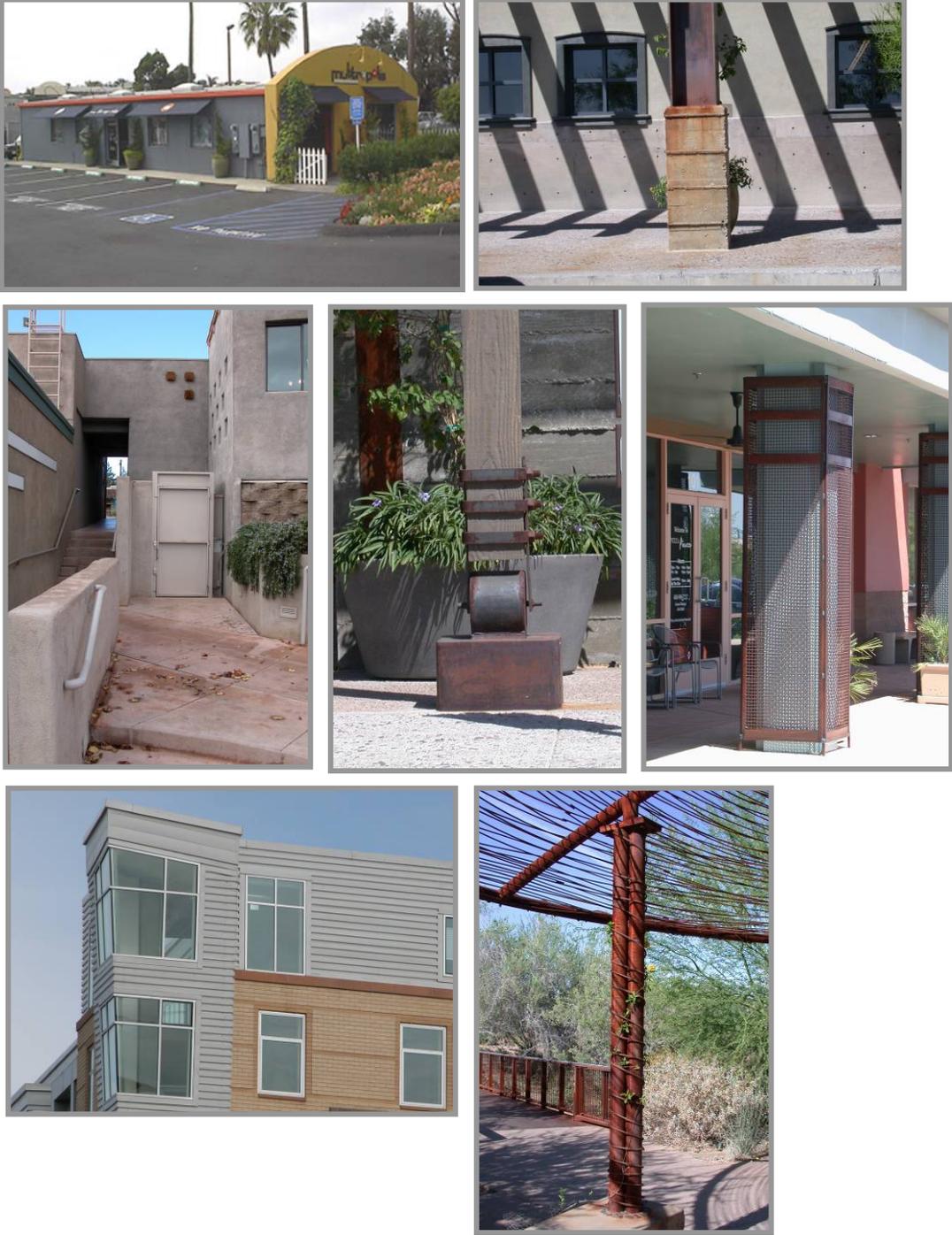
Emerging from the 1920s, these styles include curved and rounded corners, smooth wall surfaces, flat roofs, and a strong horizontal emphasis through balustrades, detail, and color. Art Deco emphasizes vertical elements via towers and other projections and often also includes zigzags, chevrons, and other geometric and stylized motifs. Elements of these styles are appropriate in all the land use subdistricts, although they are most strongly encouraged in the Old Town Highway Commercial Subdistrict.



Design Guidelines

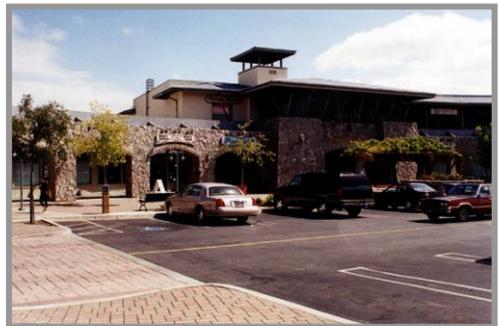
4. Artisan Industrial/Green

While not a recognized architectural style, the intent of Artisan Industrial is to encourage creativity and detail on typical large industrial development. Characterizing elements are steel, concrete, bright accent colors, abstract shapes, and unique signage, as well as pedestrian details and characteristics.



5. Contemporary

Contemporary styles may be appropriate for new construction; however, the use of local materials and design elements from surrounding structures should be considered. Care should be taken to ensure the appropriate use and scale of design elements in contemporary design. Where designed in context with surrounding architecture, contemporary styles are appropriate in all land use subdistricts.



5.3.2 Building Materials and Color

The number of materials, colors, and combinations is unlimited, and the appropriateness of any given material, color, or combination for a particular building depends on a number of factors including architectural style and details, building material, building size, and building context.

- ◆ Consider exterior materials, textures, and colors that complement the architectural style or theme of a building, as well as those indigenous to the area, as desirable.
- ◆ Avoid exterior materials such as textured plywood and paneling, fake stone veneer, plastic siding, heavy troweled finishes, and similar materials.
- ◆ Vary materials to provide architectural interest; however, limit the number of materials and colors and do not exceed what is required for contrast and accent of architectural features.
- ◆ Relate exterior materials to each other in ways that are traditional and logical.
- ◆ Colors and materials should be durable and weather-resistant; stain and flat paints are encouraged, while glossy paints and finishes are strongly discouraged.
- ◆ Where appropriate, building colors should reflect the basic colors of the architectural style or period of the building; historic color palettes based on research, old photographs, and historic records are strongly encouraged.
- ◆ The use of corporate signature color schemes on buildings is strongly discouraged.
- ◆ Avoid fluorescent, garish colors.

Recommended Materials

- Dimensioned cut stone (local)
- Board and batten (mainly parapet and upper storefront)
- Ceramic tiles (bulkhead only)
- Stucco
- Horizontal wood siding
- Treated wood shake roofs
- Nonreflective metal roofs
- Clay tile roofs
- Corrugated metal (awnings and architectural details only)
- Recycled/Green

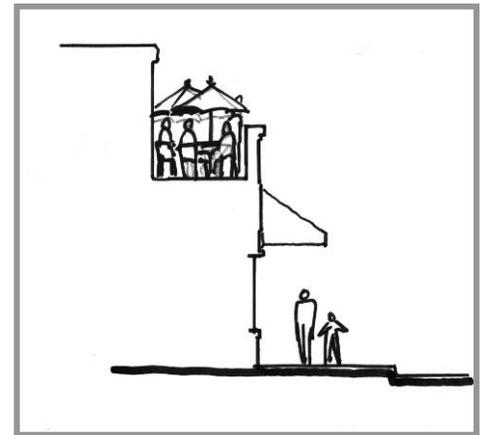
Discouraged Materials

- Faux masonry (e.g., concrete block)
- Highly reflective or opaque glass
- Imitation or concrete flagstone
- Used brick with no fired face
- Imitation wood siding
- T-111 siding
- Plastic panels
- Crushed stone roofs
- Shake roofs
- White, black, brightly colored, or reflective roofs
- Brightly colored (orange, blue, etc.) tile roofs
- Corrugated fiberglass roofs

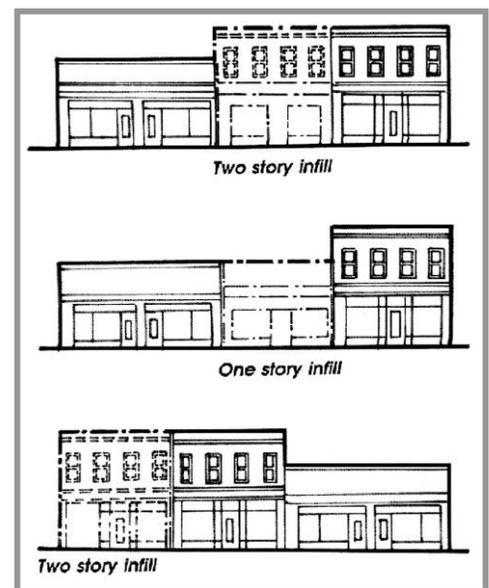
5.3.3 Building Form, Mass, and Scale

The building form, mass, and scale are important in maintaining the quality of the built environment and in protecting the scenic views of the natural environment of Yucca Valley. New construction should be compatible with surrounding buildings in terms of mass, scale, and height.

- ◆ Although building heights will vary, generally, taller buildings are encouraged in the Old Town Mixed-Use Subdistrict to create a stronger presence on the street and to frame the scenic views.
- ◆ Keep the scale of building elements, especially at the ground level, intimate and close to human scale; reduce building bulk by dividing larger building masses into smaller scale parts.
- ◆ Reduce building mass on structures with two stories by “stepping back” upper stories, which can provide for attractive balconies and outdoor spaces.
- ◆ In infill development, respect the relationship of height-to-width proportions of existing facades; “transition” new development from the height of adjacent buildings, and locate window lines and other horizontal elements to achieve a consistent pattern between buildings.
- ◆ Large, undifferentiated wall planes are strongly discouraged; express significant horizontal and vertical articulation on all sides of a building visible from public right-of-ways through:
 - Variation of roof eave line or roof structure,
 - Changes in the wall plane,
 - Full roofs with overhang,
 - Recessed entries and doors,
 - Covered walkways, trellises, and/or architectural awnings, and
 - Architectural details consistent with architectural style.
- ◆ Establish a strong connection between the ground and the site through the use of low planter walls, trees and shrubs, railings, veneer banding, and other materials and textures.



“Stepping back” upper stories reduces building mass and provides for balconies and terraces.



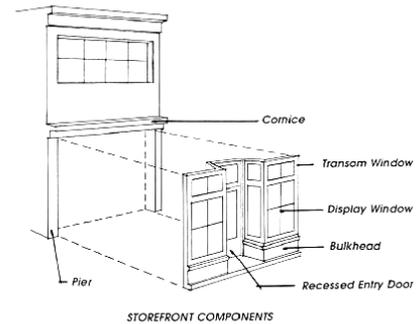
Infill development should be compatible with the height of adjacent buildings.

Design Guidelines

5.3.4 Storefront Design

Although the storefront is only one of the architectural elements of a facade, in large part, it is one of the most important elements. There are a number of design elements that can help achieve a traditional storefront design that enhances the experience of the pedestrian, yet also meets important functional needs.

- ◆ Treat each storefront like a small building, with its own base, roofline, and door and window pattern.
- ◆ For buildings on corners, include storefront design features for at least 50% of the wall area on the sidewall.
- ◆ Generally, base storefronts on modules of approximately 25 feet in width. For new buildings that are wider than existing facades on the street, break them down into a series of “structural bays” or storefronts segmented by a series of columns or piers.



Storefront widths should be based on a consistent module of approximately 25 feet.

- ◆ Building recesses are encouraged to define entryways and window openings along a building’s exterior and to provide weather protection and a transition zone from sidewalk activity into the store. Recommended treatments are special paving materials such as ceramic tile or brick, ornamental ceilings such as coffering, and decorative light fixtures.
- ◆ To reduce glare, recess windows and large areas of glass in deep shadow, and inset glass a minimum of 3 inches from the exterior wall surface to add relief. Clear glazing is strongly recommended, while reflective glazing and tinting are discouraged.



Building recesses help mark entrances.

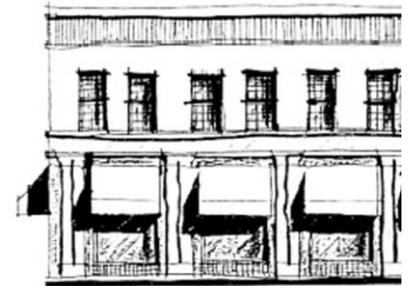
- ◆ Clearly articulate entries to commercial structures, with substantial, well-detailed doors that match the materials, design, and character of the display window framing. Doors to retail shops should contain a high percentage of glass to display retail contents.
- ◆ Providing rear pedestrian entrances via alleys and parking lots is encouraged. Improvements to rear facades should be subtle and modest, and may include signs, landscaping, and awnings.



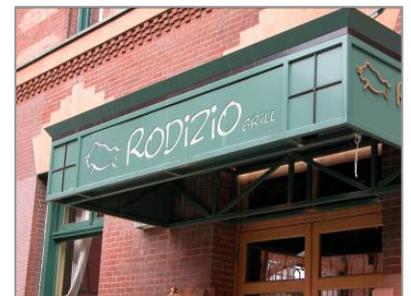
Rear entrances should be subtly enhanced.

Awnings

- ◆ Awnings may be used to protect pedestrians, add interest and color to buildings, and to accommodate pedestrian-oriented signs.
- ◆ Awning placement should respond to the scale, proportion, and rhythm of the building's design. Awnings should not cover piers, pilasters, transom windows, or other architectural features.
- ◆ Awnings should be of either canvas or acrylic-coated canvas, and not aluminum, vinyl, or backlit awnings which detract from the character of Old Town. Awning frames and supports should be of painted or coated metal or other noncorroding material.
- ◆ Projecting metal awnings that are a structural part of a building may also be appropriate.
- ◆ Retractable awnings are recommended because they are functional with a frame and support structure that can be adjusted up or down, depending upon conditions.



Awnings add visual interest.



Awnings and canopies also provide protection for pedestrians.

Design Guidelines

5.4 Development Details

5.4.1 Landscaping

Landscaping should be an integral part of the overall design concept. A carefully planned landscape can serve more than one function for the site as well as for the streetscape. The landscaping and public spaces within Yucca Valley add character as well as provide a functional purpose.

Landscaping not only softens the built environment but preserves and restores the scenic qualities of the natural landscape by retaining and/or re-vegetating areas with native plant species. The following guidelines will help ensure that landscaping is used appropriately in Old Town Yucca Valley.

- ◆ Landscaping should enhance the quality of development by framing and softening the appearance of buildings, enhancing the overall image, screening undesirable views, and providing shade and wind protection.
- ◆ Landscaped parking areas are encouraged to avoid direct views of parked vehicles from public right-of-ways, and to minimize noise, light, exhaust fumes, and other negative effects on pedestrians.
- ◆ Place landscaping so that it does not interfere with the lighting of the project area or restrict access to utilities (such as electrical boxes) or emergency apparatus (such as fire hydrants or fire alarm boxes). Keep landscaping in scale with adjacent buildings and of appropriate size at maturity.
- ◆ Use trees and plants appropriate to Yucca Valley and the High Desert Region, and that have low maintenance. Group plants in combinations to support desired design themes and based on common environmental conditions, such as soil type, water, sun, temperature, and precipitation, etc.
- ◆ Placing planters and pots in building recesses and adjacent to blank walls is encouraged as they provide visual interest and color accents, and enrich sidewalks, courtyards, and plazas. Planter and pot materials should complement the building's architectural design.



Landscaping softens building walls.



Landscaping can provide attractive buffers.



Planter pots enhance pedestrian areas.

5.4.2 Public Spaces and Site Amenities

Public spaces include plazas, courtyards, pedestrian paseos, and gardens that are designed with public amenities and landscaping to provide shade, an opportunity to rest, adequate lighting, and relief from traffic noise.

- ◆ Incorporate courtyards, plazas, outdoor eating areas, mid-block pedestrian paseos, and other amenities into development designs.
- ◆ Make public spaces visible from the street or link them to the street via a clear circulation element, such as an open passage or covered arcade.
- ◆ Make site furniture (chairs, tables) simple in design so as to not detract from the surroundings. Design site furniture to maximize shade areas, yet also maintain overall site visibility. Wind-resistant umbrellas are encouraged to create shade areas and to minimize heat build-up.
- ◆ Decorative paving that adds visual interest and a sense of place is encouraged in public spaces.
- ◆ Locate ATM machines in an area that does not visually detract from the site or impair pedestrian circulation; incorporate an attractive design in the unit housing the machine.
- ◆ Interpretive signs may be incorporated as an integral part of the design of pedestrian spaces, using design features that highlight the area's history and historical and natural environment.
- ◆ Public art and design features (interpretive features and signs, sculptures, etc.) that invite participation and interaction in public spaces are encouraged. Design features should add local meaning; interpret the local culture, environment and/or history; and capture or reinforce the unique character of place.
- ◆ Murals may be considered on building walls that do not face the primary sidewalk (e.g., may be appropriate on walls that face alley, rear parking areas, mid-block pedestrian paseos, and select pedestrian plazas).



Opportunities for outdoor dining should be provided for where possible.



Mid-block paseos are encouraged.



Wind-resistant umbrellas offer shade and comfort in public spaces.

Design Guidelines

5.4.3 Lighting

- ◆ Use low-level, decorative lighting to provide appropriate nighttime visibility for safety and pedestrian movement as well as accent detail.
- ◆ Where appropriate, design down-directed, exterior lighting as part of the overall architectural style of the building that highlights interesting architectural features. Lighting of full facades or roofs is discouraged.
- ◆ Lighting should not produce glare or spill over onto adjacent properties; consider the latest technical and operational energy conservation concepts in lighting designs.



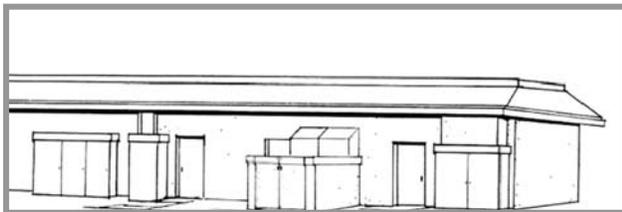
Low-level decorative lighting is appropriate.

5.4.4 Building Equipment and Services

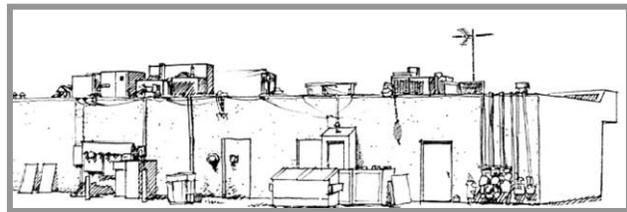
- ◆ Locate refuse, storage, and equipment out of view from the public and/or screen it to the degree possible. Make screening devices compatible with the architecture, materials, and colors of the building(s) and incorporate creativity, art, and landscaping to enhance the desired character of the area.
- ◆ If all rooftop equipment is visible from the ground or upper stories of adjacent buildings, screen it well from view, using architectural materials and colors that are consistent with related buildings.



Trash enclosures should be attractively screened.



Screening is encouraged.



Visible equipment, storage, and refuse are discouraged.

5.5 Commercial Signs

Signs are important, not only because they communicate something about goods and services being offered at a particular establishment, but also because they communicate something about the quality of the businesses and the image of the community in general. Attractive, creative, and pedestrian-oriented signs will help create a more pleasing visual environment in Old Town Yucca Valley. The following section is meant to supplement, not supersede, the adopted City of Yucca Valley Sign Ordinance.

5.5.1 Sign Materials and Colors

Sign materials and colors should complement the materials and colors on the facade and the architectural style of the building.

- ◆ Appropriate sign materials may include:
 - Wood (carved, sandblasted, etched, properly sealed and painted or stained),
 - Tile (painted, sealed, inlaid tiles),
 - In certain cases, metal (formed, etched, cast, engraved, and properly primed or factory coated to protect against erosion),
 - Stucco, and
 - Decorative iron brackets or wood (preferred for sign hardware support).
- ◆ The selected materials should contribute to the legibility of the sign; for example, glossy finishes are often difficult to read because of glare and reflections.
- ◆ Sign materials should be very durable; paper and cloth signs are not suitable for outside use because they deteriorate quickly.
- ◆ Neon signs are appropriate if they are consistent with the historical character of the area in which they are located.
- ◆ Too many colors on a sign overwhelm the viewer's ability to process quickly what the sign is communicating. Limit use of accent colors to increase legibility, and limit colors to no more than three on a single sign.
- ◆ Contrast is an important influence on the legibility of signs; the most aesthetic and effective graphics are produced with light-colored letters and images on a dark, contrasting-colored background.



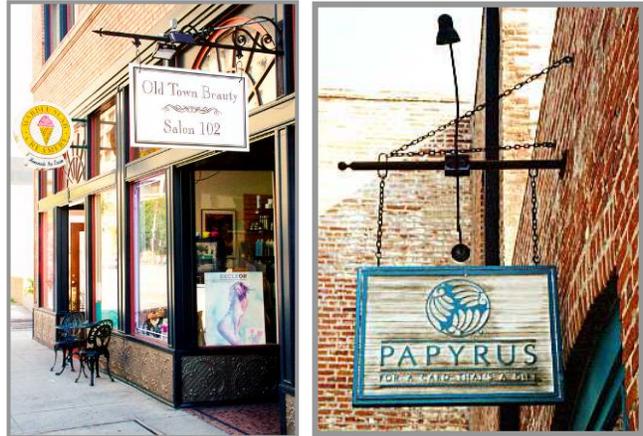
Design Guidelines

5.5.2 Encouraged Sign Types

Roof signs, pole signs, and internally illuminated box signs are discouraged. The following sign types are encouraged for new projects in Old Town Yucca Valley.

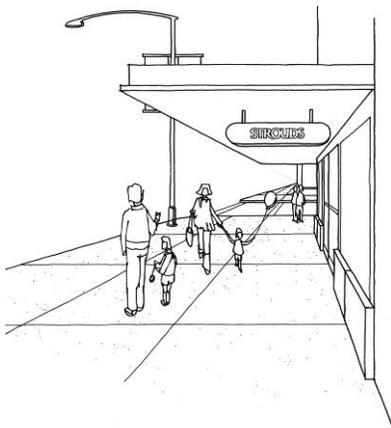
Projecting Signs

- ◆ The use of small, pedestrian-oriented signs is strongly encouraged.
- ◆ Use projecting signs for ground floor uses only; on a multi-storied building, suspend the sign between the bottom of the second story windowsills and the top of the doors or windows of the first story. On a one-story building, position the top of the sign in line with the lowest point of the roof.
- ◆ The scale of projecting signs should not detract from the architectural character of the building.
- ◆ Hang projecting signs at a 90-degree angle from the face of the building, and use sign supports and brackets that are compatible with the design and scale of the building. Decorative iron and wood brackets are encouraged.



Hanging Signs

- ◆ Where overhangs or covered walkways exist, pedestrian-oriented hanging signs are encouraged. Hang signs over the pedestrian right-of-way, consistent with the City Code.
- ◆ Keep hanging signs simple in design and avoid using them to compete with existing signage at the site, such as wall signs.



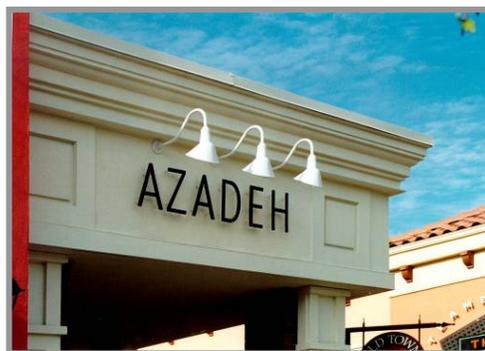
Window Signs

- ◆ Window signs should be primarily individual letters placed on the interior surface of the window and intended to be viewed from outside, with white and gold-leaf paint being the recommended colors. Glass-mounted graphic logos may also be applied, as long as they comply with the 25-percent limitation.
- ◆ Limit the text or sign copy of a window sign to the business name and a brief message identifying the product or service (e.g., “maternity wear” or “attorney”) or pertinent information (e.g., “reservations required”).



Wall Signs

- ◆ Locate a wall sign where the architectural features or details of the building suggest a location, size, or shape for the sign. The best location is generally a band or blank area between the storefront and the parapet.
- ◆ Wall signs should not project from the surface upon which they are attached more than that is required for construction purposes and, in no case, more than 6 inches.
- ◆ Place new wall signs for individual businesses in a shopping center consistent with the location of signs for other businesses in the center, which will establish visual continuity among storefronts and create a unified appearance for the center.
- ◆ For a new or remodeled shopping center, develop a comprehensive sign program for all signs in the center.



Design Guidelines

Awning Signs

- ◆ Awning signs should be placed only on the valance portion of the awning; the use of adhesive/press lettering is strongly discouraged; letter color should be compatible with the awning and the building color scheme.
- ◆ Carefully design the shape, design, and color of awnings to coordinate with, and not dominate, the architectural style of the building. Where multiple awnings are used on the building, keep the design and color of all sign awnings consistent.
- ◆ Use only permanent signs that are an integral part of the canopy or awning. To avoid having to replace awnings or paint out previous tenant signs when a new tenant moves in, consider using replaceable valances.



Monument/Freestanding Signs

- ◆ The sign area and height of the sign should be in proportion to the site and surrounding buildings; signs should not be overly large so as to be a dominant feature of the site.
- ◆ Place monument and freestanding signs perpendicular to the street so that sight lines at entry driveways and circulation aisles are not blocked. Monument signs are rarely appropriate in the Old Town Mixed Use Subdistrict.
- ◆ Design monument and freestanding signs to create visual interest and complement their surroundings, incorporating architectural elements, details, and articulation.
- ◆ Incorporate materials and colors into the sign support structures that match or are compatible with materials and colors of the development the sign serves so it does not appear out of scale with its adjacent building(s).



Chapter

6

ADMINISTRATION &
IMPLEMENTATION

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CHAPTER 6: ADMINISTRATION AND IMPLEMENTATION

The development and/or redevelopment of Old Town Yucca Valley Specific Plan project area will be a multi-year effort. The preferred land use development concept and associated improvements necessary are envisioned to occur over a 20-year period. Therefore, future development and/or redevelopment in the Specific Plan area will be dependent upon and responsive to prevailing market conditions, making it challenging to forecast the timing and extent of future conditions.

6.1 Applicability

The provisions of this chapter are applicable to development activity and land use within the boundary of the Old Town Yucca Valley Specific Plan. The regulations, development standards, and guidelines as contained in the Specific Plan, shall apply in their entirety to the review of new development proposals. However, for review of proposals to modify existing development, existing site conditions may constrain the extent to which the Specific Plan development standards and guidelines can be met. Acceptable accommodations of existing development are noted where applicable.

6.2 Amendments to the General Plan

Concurrent with adoption of the Old Town Yucca Valley Specific Plan, the Town of Yucca Valley Comprehensive General Plan shall be modified by amendment to provide consistency between the General Plan and Specific Plan. The following amendments to the General Plan shall ensure consistency:

- ◆ Amending the General Plan
 - Land Use Element – designate area as Specific Plan (SP)
 - Circulation Element – Update Map to include revised circulation

6.3 Amendments to the Zoning Code and Map

The Zoning Classifications in the Old Town Yucca Valley Specific Plan area existing before adoption of the Specific Plan, including a mixture of commercial, light industrial, and residential use classifications, shall be repealed within the Specific Plan area. The zoning map shall be amended to indicate the new Old Town Yucca Valley Specific Plan zoning classification “SP,” including the OTMU, OTHC, OTIC, OTCR, and HE districts. (See Chapter 3 for more information on the zoning districts.)

Administration and Implementation

All land use regulations, development standards, and other provisions of the Old Town Yucca Valley Specific Plan in its entirety shall apply as expressly stated in this Plan. For the development criteria and regulations that are not amended or superseded by this Specific Plan, the provisions of the Town of Yucca Valley Municipal Code shall prevail.

The provisions contained in the Specific Plan constitute the primary land use and development standards for the project area. These regulations are applied in addition to the provisions as set forth in the Town of Yucca Valley Municipal Code. As part of the implementation of the Specific Plan, the Town of Yucca Valley Municipal Code shall be amended to include the Old Town Yucca Valley Specific Plan ("SP").

6.4 Administration and Enforcement

It shall be the duty of the Town of Yucca Valley to enforce the provisions set forth in the Old Town Yucca Valley Specific Plan. All officers, employees, and officials of the Town of Yucca Valley who are vested with the duty or authority to issue permits or licenses shall comply with the provisions of the Old Town Yucca Valley Specific Plan, and shall not issue any permit or license or approve any use or building that would be in conflict with the Old Town Yucca Valley Specific Plan. Any permit, license, or approval issued that is in conflict with the requirements of the Old Town Yucca Valley Specific Plan shall be considered null and void.

6.5 Amendments to the Specific Plan

The Old Town Yucca Valley Specific Plan may be amended in the same manner by which it was originally adopted. In addition, an amendment shall demonstrate that it meets the intent of the Specific Plan's goals and objectives, or provide a finding that the amendment enhances the Plan or is necessary to implement the Plan's goals and objectives. All sections or portions of the Specific Plan to be changed or that may be affected by the change shall be included in the Specific Plan Amendment. A concurrent amendment of the General Plan is not required, provided the City Council determines that any substantive changes would not conflict with the goals, objectives, policies, or programs of the Town of Yucca Valley General Plan.

6.6 Development Review Procedures

Section 4.13 of the Old Town Yucca Valley Specific Plan establishes the procedural and content requirements for the review and approval of specific development projects within the Old Town Yucca Valley Specific Plan area.

6.7 Severability

If any portion of the Old Town Yucca Valley Specific Plan is, for any reason, held invalid by a court of competent jurisdiction, such portion shall be deemed a separate, distinct, and independent provision, and the invalidity of such provision shall not affect the validity of the remaining portion of the Old Town Yucca Valley Specific Plan.

6.8 Development Strategy

Two complementary strategies have been devised to implement the improvements under the Old Town Yucca Valley Specific Plan: a long-term strategy and a short-term strategy. These strategies are differentiated by the eventual realignment of the SR-62. The long-term strategy includes all of the proposed improvements under the Specific Plan including the realignment of SR-62. The short-term strategy does not include the highway realignment and only involves interim improvements that eventually complement the long-term strategy.

Short-Term (Pre Highway Realignment)

- ◆ Adoption and promotion of Specific Plan and Program Environmental Impact Report (EIR)
- ◆ Establish an Old Town Management Organization involved in the enhancement of Old Town, including maintenance, safety, upkeep, cleanliness, and fiscal management
- ◆ Improvement of streetscapes and building facade, pedestrian environment, and directional signage that complement the eventual highway realignment
- ◆ Acquisition of strategic parcels to enable new development
- ◆ Promotion of Old Town to developers
- ◆ Promotion of assets and events in the Old Town to local residents and the tourist market

Long-Term (Full Project Implementation)

- ◆ Realignment of SR-62, enabling contiguous development of the Old Town district
- ◆ Establishment of Main Street, place making and Old Town identity
- ◆ Pedestrian and traffic improvements
- ◆ Streetscape and façade improvements
- ◆ Provision of adequate public facilities, amenities and services
- ◆ Provision of visitor services

6.8.1 Development Implementation

The following key steps are recommended for the implementation of the long-term and short-term development strategies:

Short-Term (Pre Highway Realignment)

The time frame for the realignment of SR-62 is between 6 and 10 years. In the interim period, there are a number of revitalization projects that can be implemented throughout Old Town Yucca Valley. The following short-term projects are intended to serve as immediate catalysts for revitalization, while also complementing the eventual realignment of SR-62.

- ◆ *Adopt and Promote Specific Plan and Program EIR.* To initiate the revitalization efforts in Old Town Yucca Valley, the Specific Plan and accompanying Program EIR need to be adopted by the Yucca Valley Town Council and then publicized through the reproduction and distribution of the Vision Plan posters.

Administration and Implementation

- ◆ *Establish and Fund an Old Town Management Organization.* Downtowns need a management organization with a paid Executive Director, Board of Directors, bylaws, etc. to coordinate activities. The Town of Yucca Valley, working in collaboration with the Yucca Valley Chamber of Commerce and the Old Town Merchants Association, should consider either establishing a new public-private-non-profit partnership [501(c)3 organization, preferably] or expanding the existing Association to focus on recruiting community support, educating business and property owners, and assisting in the implementation of elements of the Specific Plan. The already established Old Town Merchants Association can take a large role as either a member of the public-private-non-profit partnership or as the actual organization that is evolved into the Old Town management organization.
- ◆ The organization should have a paid Executive Director (preferably a Certified Main Street Manager or a professional downtown manager with at least 5 years experience). Because starting a downtown management organization takes considerable energy and focus by the Executive Director, a commitment of at least 5 years salary and benefits should be budgeted for the position. The organization can be set up similar to a Main Street organization with committees for promotion, organization, economic development, and design. Within each of these committees there is a focus on implementing specific aspects of the Specific Plan. For example, the economic development committee can assist in identifying Old Town markets and niches, as well as retaining existing business and recruiting new businesses.
- ◆ *Improve Streetscape and Pedestrian Environment.* The Town should install missing sidewalk segments throughout Old Town and install benches, trash receptacles, light standards, and other non-permanent items from the Old Town Yucca Valley streetscape furniture palette. This will improve the pedestrian areas throughout the Old Town core and the amenities can be easily moved after the SR-62 realignment. Additionally, alley improvements north and south of Main Street between, Cherokee Trail and Fox Trail should be implemented to reinforce a friendlier pedestrian environment. Coordination with Caltrans may be necessary for some streetscape enhancements.
- ◆ *Install Wayfinding Direction Signage.* To assist motorists and pedestrians in the promotion and recognition of Old Town's assets, new wayfinding signage should be installed to assist visitors and patrons in finding Old Town businesses, parking areas, and other amenities.
- ◆ *Improve Facades.* Façade improvements could be started on existing structures, along with creating an incentive such as a façade grant to assist with the costs.
- ◆ *Acquire Strategic Parcels:* The Town of Yucca Valley, or the redevelopment agency, can begin to acquire parcels near Elk Trail, north of Main Street (existing SR-62) to create an Old Town Center with centralized public plaza and community amenities. A post-office branch, museum, community center, performing arts center, senior center, or Town library should be encouraged in this area. The public uses could be combined with retail or office space to add to the mixed-use environment sought for Old Town. This

Old Town Center will provide a much needed space for public gatherings such as street fairs or a farmers market.

- ◆ *Promote Old Town to Developers.* New compatible infill development should be encouraged throughout Old Town. This development should be sought after by the public-private-non-profit partnership or the Old Town Merchants Association once the Town has acquired strategic parcels and the Specific Plan is adopted.
- ◆ *Promote Old Town Assets and Events.* Old Town Yucca Valley has many existing assets. Promoting Old Town during and after the revitalization and implementation of the Specific Plan is very important to gain and maintain the momentum of support in Old Town through increased sales by locals and visitors. Key promotional events along with advertising should be implemented.

Long-Term (Full Project Implementation)

Upon realignment of SR-62, the following implementation strategies should be considered:

- ◆ *Realignment of SR-62, enabling contiguous development of the Old Town district.* The realignment of SR-62 includes a semi-grid system of roadways, emphasizing community and regional linkages to the Old Town area. This will enable ease of pedestrian and vehicular movement through and around the Old Town Core, providing a greater area for key new development, redevelopment and infill of properties within the area.
- ◆ *Establishment of Main Street, place making and Old Town identity.* To create a unique identity within Yucca Valley, a “Main Street” is needed within the existing SR-62 alignment extending through the center of Old Town. The Main Street design incorporates an enhanced gateway from SR-62 creating a sense of entry into a place. To promote a different feel from SR-62 many traffic calming measures will be implemented to reduce traffic speeds, enhance pedestrian safety and promote walkability of the area.
- ◆ *Pedestrian and traffic improvements.* Developing a logical system of linkages and connections to and between the Old Town area and other areas of the Town for bicyclists, pedestrians, and automobiles will contribute to Old Town’s revitalization; at the core of this approach are SR-62 realignment and improvement of the local street system.
- ◆ *Streetscape and façade improvements.* To further enhance and create a unique identity for Old Town, streetscape and façade improvements will be implemented. The streetscape improvements include an integrated design of amenities and landscaping such as benches, trash receptacles, and desert plants. The façade improvements will reflect the character of the district in which the building is located as well as its use.

Administration and Implementation

- ◆ *Provision of adequate public facilities, amenities and services.* To ensure the sustainability of future development and revitalization of Old Town Yucca Valley, public facilities, amenities, and services need to be upgraded and provided for the future build-out potential of the area. Some of these services include water, wastewater, and storm water management as well as adequate fire and police protection.
- ◆ *Provision of visitor services:* There are services that need to be provided to new visitors to Old Town Yucca Valley that allow for a pleasant visit and encourage a return trip. Visitor services should include a wayfinding signage system that directs pedestrians to districts and drivers to parking options. A sister visitor center to the California Visitor Center in the center of Yucca Valley is needed to provide information for first time visitors to Old Town Yucca Valley and the surrounding county.

6.9 Financing Strategy

The financing strategy recommended for undertaking the improvements in the Specific Plan are in keeping with the two development scenarios.

Short-Term (Pre Highway Realignment)

Public: Financing of interim capital improvements including streetscape and façade improvements through redevelopment property tax increment and community development block grants. Provide seed money for the establishment of a property-based business improvement district.

Private: Operations and maintenance, Old Town promotion, and limited capital improvements through the business improvement district and the expanded or reconstituted Old Town Merchants Association. Continuing private sector investment in upgrading and developing Old Town properties.

Long-Term (Full Project Implementation)

Public: Financing of capital improvements through redevelopment, community grants, transportation funds, and other special grants.

Private: Includes development impact fees and benefit assessments, as well as potential special taxes through the establishment of special districts for both capital costs, and some operations and maintenance costs.

6.9.1 Financing Implementation

The following key steps are recommended for the implementation of the long-term and short-term development strategies. The steps identified are intended for consideration and have not specifically been authorized at this time.

Short-Term (Pre Highway Realignment)

- ◆ Commence process to adopt the Old Town Specific Plan and development vision.
- ◆ Strengthen public-private partnership mechanism for the promotion of Old Town.

- ◆ Establish an Old Town Merchants Organization or expand the existing Old Town Merchants Association to facilitate operations and maintenance of the Old Town.
- ◆ Establish property based business improvement district.
- ◆ Consider raising the citywide transient occupancy tax (TOT), with a portion dedicated to the improvement of visitors and tourists related services.
- ◆ Use available redevelopment property tax increment and community development block grants for targeted improvements. Incorporate the specific plan improvements into the Town's capital improvement program (CIP) process.

Long-Term (Full Project Implementation)

In addition to the above interim steps, the following long-term steps are recommended:

- ◆ Implement full complement of improvements and amenities through increase in the property tax increment.
- ◆ Complete the realignment of SR-62, with the majority of the financial support assumed from Town-led efforts to secure grants or other appropriations from federal, state, and other sources.
- ◆ Prepare long-term CIP and pursue available grants and loans (e.g. TEA 21, EDA etc.).
- ◆ Devise and implement assessments and/or special taxes, as approved by local property owners.

6.10 Potential Funding Sources

Potential public improvements include new street trees, grates, lighting in designated areas, benches, trash receptacles, median landscaping, traffic islands, roadway and transit improvements, on and off-street parking, sidewalk improvements, public art and signage. These enhancements are intended to define the Specific Plan area and its relationship with adjacent areas as well as to create a pedestrian friendly environment that links civic, business and residential land uses together.

This section describes potential funding sources that could be used for implementing the improvements as well as supporting some annual operations and maintenance costs. The sources identified are intended for consideration and have not specifically been authorized at this time. *Table 6-1 Financing Techniques and Types of Programs* presents a summary of the potential funding sources that are described in more detail below.

6.10.1 Funding Sources for Capital Facilities

The potential funding sources available to the Town have been listed below by the categories of: infrastructure, business assistance/attraction, redevelopment, grants and loans, and public art and streetscapes. Some of these sources are currently in use by the Town while others are potentially available. The bulk of the potential realignment of SR-62 is assumed to be funded by CalTrans or other external sources. Some local matching funds would also be made available.

Table 6-1
Old Town Yucca Valley Specific Plan
Financing Techniques and Types of Improvements

Financing Technique	Funding Type	Capital	Operations and Maintenance
General Fund ¹	Public		X
Gas Tax	Public	X	X
Development Impact Fee	Private	X	
Quimby Fees	Public	X	
Capital Improvement Program	Public	X	
AB 2928 Congestion Relief	Public	X	
Business Improvement District			
Business-based	Private	X	X
Property-based ²	Private	X	X
Redevelopment	Public	X	
Transportation Equity Act (TEA 21)	Public	X	
Community Development Block Grants	Public	X	
General Obligation Bond	Public	X	
California Infrastructure and Development Bank	Public	X	
Landscape and Lighting District	Private	X	X
Mello Roos Community Facilities District	Public	X	X
Donor Programs	Private	X	X
Sustainable Building Grants	Public	X	
Benefit Assessments	Private	X	

1. General Fund includes a variety of discretionary revenue sources, and while these may be used for capital expenses, these are generally only used for capital outlays.

2. Property-based BIDs are generally used for ongoing operations and maintenance, and minor capital outlays.

Source: Stanley R.Hoffman Associates, Inc.

Federal

1. The Transportation Equity Act for the 21st Century (TEA 21)

This program has discretionary funds that are available through a grant process administered by the Federal Government through the 1998 federal legislation, the Transportation Equity Act for the 21st Century (TEA-21). Federal and state statutes require the preparation of a Transportation Improvement Program (TIP) for San Bernardino County. Through the TIP

process funding is allocated to all surface transportation modes based on requirements specified in the TEA-21 program and State requirements mandated by the California Transportation Commission. A high priority is placed on enhancing connections between highways, transit and pedestrian movement, and on integrating these systems into their surrounding communities.

The Transportation Enhancement Activities (TEA) program provides financial support for surface transportation system projects that enhance mobility or encourage quality-of-life in or around transportation facilities. These include pedestrian and bicycle oriented projects, scenic or historic highway programs, landscaping and other scenic beautification. The federal government typically pays 88 percent of an approved project's cost, with project sponsors paying the balance. Capital projects with a life of less than 20 years, or a one-time temporary improvement, are ineligible. It is anticipated that TEA 21 and TEA funds will probably be available to the Town of Yucca Valley in sufficient amounts to complete major projects in Old Town. The San Bernardino Association of Governments (SANBAG) has committed most of the funds to projects in other parts of the County.

2. Federal Economic Development Administration (EDA)

The Federal Economic Development Administration (EDA) is a potential source of grant money for the Yucca Valley Specific Plan. Funds from the EDA can be used to finance construction and rehabilitation of infrastructure and facilities that are necessary to achieve long-term growth and dynamic local economies.

State

1. AB 2928 Traffic Congestion Relief

One of the major issues in the State has been improving its transportation infrastructure. Under Assembly Bill 2928, the State established a Traffic Congestion Relief Fund in its maintenance and reconstruction funds to cities and counties based on population. These funds may be used for local street and road maintenance and rehabilitation, and transit and State transportation improvement program projects. A one-time amount was allocated to cities and counties in fiscal year 2000/01. The current State budget crisis will likely preclude the State from funding this program in the short term.

2. Statewide Propositions

The Town of Yucca Valley secures funds through Proposition 12 and Proposition 40 for the upkeep of parks and recreational areas. Proposition 12 allows the State to sell \$2.1 billion of general obligation bonds to spend on protection of land around lakes, rivers, streams and the coast to improve water quality and ensure clean drinking water; to protect forests and plant trees to improve air quality; to preserve open space and farmland threatened by unplanned development; to protect wildlife habitats; and to repair and improve the safety of state and neighborhood parks. Proposition 40 allows the State of California to raise \$2.6 billion through the sale of general obligation bonds for development, restoration, and

Administration and Implementation

acquisition of state and local parks, recreation areas and historical resources, and for land, air, and water conservation programs.

3. Infrastructure State Revolving Fund Program of the California Infrastructure and Economic Development Bank (CIEDP)

This is a loan program that provides low-cost financing to public agencies for a variety of infrastructure programs, including: streets, bridges, drainage, water supply, flood control, environmental mitigation measures, sewage collection and treatment, solid waste collection and disposal, water treatment and distribution, educational facilities and parks and recreational facilities. Funding assistance ranges from \$250,000 to \$10,000,000. The application process is complicated and slow. There must be a dedicated source for debt service of the loan. Tax increment flowing from redevelopment projects is often favored as a funding source for retiring this debt because it flows for a long time and is steady. The term of the loan can be as long as twenty years. If started soon enough, this mechanism could be a very useful tool for constructing or providing matching funds for a major improvement such as a freeway interchange or overpass.

4. State Gasoline Taxes/Operations and Maintenance

The Town receives State gasoline taxes that may be used for operating and maintenance expenditures related to streets and roads. While these funds are limited, the Town's annual budgeting process may designate a portion of these revenues for specific facilities within the Old Town area subject to annual budgeting priorities.

5. Sustainable Building Grants

This grant is available from the California Integrated Waste Management Board to provide local, public funding for projects that advance the use of green building design and construction practices. There are no matching requirements and the amount available has ranged from \$50,000 to \$100,000. It is recommended that these grants be studied closely for their potential to undertake smaller building and retrofitting projects in Old Town.

6. Renewable Energy Loan Guarantee Program

This is a loan guarantee program provided by the California Trade and Commerce Agency. The loan guarantees are available to businesses that need financial assistance to purchase and install renewable energy systems.

7. Downtown Rebound Capital Improvement Program

This is a loan program available through the State's Business, Transportation and Housing Agency. The loans are used to finance the conversion of vacant or underutilized commercial and industrial structures into residential units. Currently there are no funds available, but funding may become available at a later time.

8. Downtown Rebound Planning Grants Program

This is a loan program available through the State's Business, Transportation and Housing Agency. The loans are used to fund local planning for infill housing, adaptive reuse of commercial and industrial space into residential units, and the development of other forms of high-density downtown housing. Currently there are no funds available, but funding may become available at a later time.

Local and Regional

1. **Redevelopment Agency (RDA) Tax Increment**

Old Town is located within the boundaries of the Town's merged Redevelopment Project Area. The Redevelopment Agency budget for 2005/06 indicates that estimated property tax increment is not available for funding any projects outside of affordable housing. In the Spring of 2004, the Redevelopment Agency completed a tax allocation bond issue in the amount of \$6.8 million. A large amount of the issue was used to retire an older bond issue. However, the City has set aside \$1.7 million of the proceeds to be used in supporting the implementation of the Old Town Specific Plan when it is complete. The Town has not currently allocated these funds to projects, but is waiting for more definition in the Old Town Specific Plan and implementation opportunities. The funds could be used for land banking, public capital improvements, or other projects or purposes that would support implementation of the Plan.

2. **Redevelopment Agency (RDA) Housing Set Aside**

State redevelopment law requires that the redevelopment agency set aside 20 percent of any property tax increment revenues for low- and moderate-income housing needs. Currently, the RDA budget is primarily allocated for the funding of affordable housing projects.

3. **Future Redevelopment Project Revenues**

The future for the generation of new tax increment in the merged redevelopment project area is very strong. Major retail projects are expected to receive their respective entitlements in coming months and begin construction shortly thereafter. Together with other large projects, a growing tax increment is forecast for the combined projects. The generation of increased increment means that additional tax allocation bonds can be sold at a future time to augment the implementation of the Old Town Specific Plan.

4. **Transient Occupancy Tax**

The Transient Occupancy Tax (TOT) – a General Fund revenue source – is levied on hotel operators whose properties are located within the boundaries of the Town. The Town of Yucca Valley currently has a 7 percent TOT rate on the relatively few hotel and motel rooms in the Town. Tax is levied on each occupied room for each night's use as a percentage of the room rate. The tax is remitted directly to the Town. The number and size of these commercial properties are expected to grow substantially in the future and the Town could consider raising the TOT to further enhance this revenue source. A portion of this tax could be directed toward promotional and tourist-related activities benefiting the Old Town.

5. **Business Improvement Districts (BID)**

The Parking and Business Improvement Area Law of 1989 provides the legal basis for what is commonly known as a business improvement district (BID). This type of BID utilizes an annual

Administration and Implementation

assessment levied against business owners based on a measure of benefit to each participating business, and can support a limited range of ongoing activities, including streetscape and sidewalk improvements, trash and cleanup activities, promotion and advertising, and public safety. Special assessments within this type of business improvement district cannot be used to secure loans or bonds, thus limiting financing strategies to a pay-as-you-go basis. Assessments must be directly proportional to the estimated benefit to be received and are levied on businesses in a specified area.

The Property and Business Improvement District Law of 1994 also provides funding for a wide range of improvements and activities. Assessments are levied against property owners rather than businesses, a key difference from the business-based Parking and Business Improvement District Law of 1989, as previously described. Property owners are often more willing to assess themselves if the result may directly benefit their investment in real property. Any assessments must be re-authorized by a majority vote in proportion to their assessment every five years. Compared with a business-based business improvement district that requires annual reinstatement, this longer five-year period also allows for minor capital improvements to be funded through loans. Assessments must be directly proportional to the estimated benefit to be received and are levied on property owners in a specified area. Such formation would not have to wait for the SR 69 project to start but could be organized and operating well within the time frame for the commencement of the actual construction of the bypass.

In California, there are two different types of business improvement districts, one created through assessments on business licenses within the district (including landowners who lease property), and the second created through assessments of property owners alone. Both forms of districts can undertake a wide variety of programs, including, but not limited to the following:

- ◆ Fountains, benches and trash receptacles, and integrated signing
- ◆ Street lighting
- ◆ Security services that are supplemental to those normally provided by the municipality.
- ◆ Special cleaning operations, graffiti removal, and waste management
- ◆ Decorations and public art
- ◆ Promotions of public events benefiting area
- ◆ Furnishing music to any public place in the area
- ◆ Promotion of tourism within the area (only businesses benefiting from tourist visits can be assessed for this type of benefit)
- ◆ Any other activities which benefit businesses located in the area

6. Landscape and Lighting Maintenance District (LMDs)

The Landscaping and Lighting Act of 1972 enables assessments to be imposed in order to finance the maintenance and servicing of landscaping, street lighting facilities, ornamental structures, and park and recreational improvements.

7. Mello-Roos Community Facilities District (CFDs)

Cities, counties, special districts, and school districts can establish community facilities districts (CFDs) to levy special taxes to finance a variety of facilities and services. Services that a Mello-Roos CFD may fund on a pay-as-you-go basis include: police and fire protection, ambulance and paramedic, flood protection, recreation program and library services, and additional funds for the operation and maintenance of parks, parkways, open space, museums, and cultural facilities. Special taxes require a two-thirds voter approval.

8. Parking Revenues

The use of a Parking District to construct and fund common parking facilities serving the commercial businesses has been used successfully in other downtown revitalization projects. Potential funding sources range from in-lieu fees for spaces to parking revenues from monthly parking and short-term parking fees. Old Town will need secure, accessible, well-signed, and reasonably priced off-street parking, in addition to on-street parking, as the Plan is realized and Old Town reaches its full potential.

9. General Fund

The Town's General Fund is used to support ongoing Town operations and services, including general government operations, development services, public safety, and community services. Primary revenue sources for the General Fund include property taxes, sales taxes, and intergovernmental revenues.

10. Sustainable Building Grants

This grant is available from the California Integrated Waste Management Board to provide local, public funding for projects that advance the use of green building design and construction practices. There are no matching requirements and the amount available has ranged from \$50,000 to \$100,000. It is recommended that these grants be studied closely for their potential to undertake smaller building and retrofitting projects in Old Town.

11. Renewable Energy Loan Guarantee Program

This is a loan guarantee program provided by the California Trade and Commerce Agency. The loan guarantees are available to businesses that need financial assistance to purchase and install renewable energy systems.

12. Downtown Rebound Capital Improvement Program

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and industrial structures into residential units. Currently there are no funds available, but funding may become available at a later time.

13. Downtown Rebound Planning Grants Program

This is a loan program available through the State's Business, Transportation and Housing Agency. The loans are used to fund local planning for infill housing, adaptive reuse of commercial and industrial space into residential units, and the development of other forms of high-density downtown housing. Currently there are no funds available, but funding may become available at a later time.

14. Special Benefit Assessments

Special Benefit Assessment Districts (AD) are formed for the purpose of financing specific improvements for the benefit of a specific area by levying an annual assessment on all property owners in the district. Each parcel of property within an AD is assessed a portion of the costs of the public improvements to be financed by the AD, based on the proportion of benefit received by that parcel. The amount of the assessment is strictly limited to an amount that recovers the cost of the "special benefit" provided to the property. Traditionally, improvements to be financed using an AD include, but are not limited to, streets and roads, water, sewer, flood control facilities, utility lines, and landscaping. A detailed report prepared by a qualified engineer is required and must demonstrate that the assessment amount is of special benefit to the parcel upon which the assessment is levied. Prior to creating an assessment district, the Town, county or special district must hold a public hearing and receive approval from a majority of the affected property owners casting a ballot. Ballots are weighted according to the proportional financial obligation of the affected property. There are many assessment acts that govern the formation of assessment districts, such as the Improvement Act of 1911, Municipal Improvement Act of 1913, Improvement Bond Act of 1915 and the Benefit Assessment Act of 1982, as well as other specific facility improvement acts.

The Landscaping and Lighting Act of 1972 enables assessments to be imposed to finance acquisition of land for parks, recreation and open space; the installation or construction of planting and landscaping, street lighting facilities, ornamental structures, and park and recreational improvements. In addition, maintenance and servicing of any of these may also be financed under this act.

15. General Obligation Bonds (G. O. Bonds)

General Obligation bonds may be used to acquire, construct, and improve public capital facilities and real property. However, they may not be used to finance equipment purchases or pay for operations and maintenance. G. O. Bonds must be approved by two-thirds of the voters throughout the Issuer's jurisdiction in advance of their issuance and typically require the issuing jurisdiction to levy a uniform ad valorem (property value) property tax on all taxable properties to repay the annual debt service.

16. Public Art Program

There is no authorized public art program at this time in the Town, but such a program could potentially be implemented. Typically, the programs either require a certain percentage of both public and private development to be dedicated to public art or an in-lieu public art

fee is charged. Under certain conditions CDBG and redevelopment funds could be used for this purpose.

17. Donor Programs

Some of the proposed improvements may lend themselves to a public campaign for donor gifts. Donor programs have been used very successfully in many cities in the United States for providing funds for streetscape and community design elements. Such programs can be tailored to solicit contributions from individuals, corporations, local businesses, and community and business associations. Many improvements could be funded by donor gifts for items such as: benches, trash receptacles, street trees, street tree grates, public art elements, and information kiosks. Donors could be acknowledged with a plaque on the element itself or other prominent display, such as a “wall of fame” with donor names. This type of program could be spearheaded by an active Old Town BID. It could draw from the wider community of business and residents.

18. Town Signage Program Funds

While the Town does not currently have a signage program, it does have sign regulations. Typically, funds from this program are used for banners and signs, but can also be used for the construction of entrance enhancements, such as signs or obelisks, which serve as entry statements to the Town. CDBG funds may be used to fund signage programs.

19. Façade Improvement Program

The Town does not currently have a façade improvement program. Sources of funding such as redevelopment and CDBG funding could be directed to meet the need as Old Town revitalization progresses. These programs could also be financed by the \$1,700,000 redevelopment bond proceeds that are potentially available to assist in the implementation of the Old town Specific Plan.

20. Community Development Block Grants (CDBG)

Community Development Block Grants provide eligible metropolitan cities and urban counties (called "entitlement communities") with annual direct grants that they can use to revitalize neighborhoods, expand affordable housing and economic opportunities, and/or improve community facilities and services, principally to benefit low- and moderate-income persons or neighborhoods. These funds can be used for reconstructing or rehabilitating property from housing to shopping centers, and to demolish property and clear sites to prepare the land for other uses. Also eligible are the building of public facilities and improvements, such as streets, sidewalks, sewers, water systems, community and senior citizen centers, and recreational facilities. The City receives approximately \$200,000 annually from the County on a pro rata share with other cities based upon population.

CDBG funds represent a potential source for financing eligible public improvements. These funds could be partially redirected for worthy projects in Old Town such as façade

Administration and Implementation

improvement, public pedestrian amenities, and other supporting infrastructure in addition to their current uses.

21. Local Surface Transportation (LSTP)

The Local Surface Transportation special funds were established to account for funds allocated by the San Bernardino Associated Governments to be used for construction and reconstruction of roads.

22. Measure I Countywide Funds

Measure I funds were established to collect revenue for transportation needs through the imposition of a half cent of sales tax per dollar of retail sale. The Measure I funds are comprised of 5 percent transit funds, 30 percent local road funds, and 65 percent major arterial funds.

23. Capital Improvement Program

The Town's Capital Improvement Program (CIP) is a process that establishes funding objectives, projects, and revenue sources over a five-year period. With the Old Town Specific Plan, a number of public enhancements, services, and infrastructure improvements will need to be incorporated into the updated CIP. The Capital Improvement program also integrates other priority projects and specifies funding sources throughout the Town. It provides a blueprint for successful implementation of the Old Town Plan in the context of the entire Town. The basic projects are currently limited to street and rights-of-way improvements that are funded primarily by gas taxes and traffic improvement fees.

24. Development Impact Fees

The Town assesses development impact fees for the construction of new buildings within the Town of Yucca Valley, with the exception of senior-citizen housing as defined by the United States Department of Housing and Urban Development. New residential projects are assessed the impact fee based upon the number of new units to be constructed. Commercial and industrial projects are assessed the impact fee based upon the amount of new square footage. In concept, the Town charges the development community a variety of fees, providing funds to pay for development-related capital projects. These fees do not usually cover the full cost of necessary infrastructure.

25. Quimby Fees

The "Quimby fee" provision of the Subdivision Map Act permits the Town to require that developers either dedicate parkland or pay an equivalent fee that allows the Town to buy and improve land for parks and recreation. The fee applies to residential uses and is based upon the number of units multiplied by a density factor. This fee is used to purchase land for parks in order to meet the Town's standard of 3 acres of parkland per 1,000 population. The Town levies such a fee on new development at the present time.

6.11 Implementation Matrix

This section specifies the steps and actions that should be undertaken within the next 5 years to implement the Specific Plan and revitalize Old Town Yucca Valley. The steps and actions are divided into separate sections, according to the Old Town Specific Plan: Initiation; Planning and Design; Economic

Development, Promotion, and Organization. For each step, a specific set of actions is to be undertaken. This implementation matrix represents the culmination of the entire Old Town Yucca Valley planning process. The following specific information from subsections 6.11.1-6.11.3 is located within *Table 6-2 Implementation Program & Matrix*.

6.11.1 Timeframes for Implementation

The timeframes are provided (by year) to guide and prioritize the actions to be implemented over a 5-year period.

6.11.2 Implementation Leaders and Teams

Key Town departments, other pertinent organizations, and Specific Plan stakeholders are identified to lead and/or complete each action. The following key defines the abbreviations listed in the “Leaders and Teams” section of the implementation matrix.

- **TOWN** = Town of Yucca Valley
Departments involved may include the City Manager’s Office, Planning and Community Development, Public Works Department, Building Department, Parks and Recreation Department, Finance Department, Library, and Police Department.
- **CNTY** = San Bernardino County
- **ST/CA** = State of California
Departments involved may include the California Department of Transportation (Caltrans), California Commission for Jobs and Economic Growth, and California Environmental Protection Agency (CalEPA).
- **HDWD** = Hi-Desert Water District
- **CoC** = Yucca Valley Chamber of Commerce
- **MBTA** = Morongo Basin Transit Authority
- **OTMA** = Old Town Merchants Association
- **BWF** = Basinwide Foundation

6.11.3 Potential Funding Sources Matrix Key

Funding sources are identified below and discussed within Section 6.10. Refer to Table 6-2, *Implementation Program & Matrix* for potential funding resources per Action Item.

Administration and Implementation

Federal

- F1. The Transportation Equity Act for the 21st Century (TEA 21)
- F2. Federal Economic Development Administration (EDA)

State

- S1. AB 2928 Traffic Congestion Relief
- S2. Statewide Propositions
- S3. Infrastructure State Revolving Fund Program of the California Infrastructure and Economic Development Bank (CIEDP)
- S4. State Gasoline Taxes/Operations and Maintenance
- S5. Sustainable Building Grants
- S6. Renewable Energy Loan Guarantee Program
- S7. Downtown Rebound Capital Improvement Program
- S8. Downtown Rebound Planning Grants Program

Local and Regional

- L1. Redevelopment Agency (RDA) Tax Increment
- L2. Redevelopment Agency (RDA) Housing Set Aside
- L3. Future Redevelopment Project Revenues
- L4. Transient Occupancy Tax
- L5. Business Improvement Districts (BID)
- L6. Landscape and Lighting Maintenance District (LMDs)
- L7. Mello-Roos Community Facilities District (CFDs)
- L8. Parking Revenues
- L9. General Fund
- L10. Sustainable Building Grants
- L11. Renewable Energy Loan Guarantee Program
- L12. Downtown Rebound Capital Improvement Program
- L13. Downtown Rebound Planning Grants Program
- L14. Special Benefit Assessments
- L15. General Obligation Bonds (G.O. Bonds)
- L16. Public Art Program
- L17. Donor Programs
- L18. Town Signage Program Funds
- L19. Facade Improvement Program
- L20. Community Development Block Grants (CDBG)
- L21. Local Surface Transportation (LSTP)
- L22. Measure I Countywide Funds
- L23. Capital Improvement Program
- L24. Development Impact Fees
- L25. Quimby Fees

Table 6-2
Implementation Program & Matrix

Steps Actions	Year(s) of Implementation					Implementation Leaders and Team Members								Potential Funding Resources	
	Year 1	Year 2-3	Year 4-6	Year 7-10	Year 11+	TOWN	ST/CA	CNTY	HDWD	MBTA	CoC	OTMA	BWF		Other
REDEVELOPMENT PROJECT INITIATION RECOMMENDATIONS															
I. INITIATE THE OLD TOWN YUCCA VALLEY SPECIFIC PLAN															
a. Officially Recognize the Old Town Yucca Valley Specific Plan															na
b. Reproduce & Distribute Plan & Vision Poster															L1, L9
c. Undertake Surveying and Mapping Effort in the Old Town Yucca Valley Core															L1, L9
d. Implement a Project Now!															L1, L9
II. ORGANIZE STRATEGICALLY FOR OLD TOWN YUCCA VALLEY REDEVELOPMENT/DEVELOPMENT															
a. Get Strategically Organized- Establish and Fund an Old Town Management Organization															F2, L9
i Enhance Public-Private Partnership															see above
ii Focus on Collaboration, Action, Implementation and Momentum															see above
iii Build Team, Leadership, Education and Commitment															see above
iv Engage Property Owners															see above
v Recruit Community Support															see above
vi Market the Plan to Agencies															see above
vii Seek Grants															see above
b. Create a Strategic Community Development Plan and Program															F2, L9
i Establish Explicit Criteria for Economic Decision-making															see above
ii Clarify Strategic Niches; Communicate Image															see above
iii Develop the Town of Yucca Valley Strategically as a System															see above
URBAN PLANNING & DESIGN RECOMMENDATIONS															
I. IMPLEMENT SMART DEVELOPMENT & DESIGN STRATEGIES															
a. Adopt Old Town Yucca Valley Specific Plan (SP) Zoning District															L9
b. Refine & Adopt Amendments to Zoning Ordinance (Z/O) & Comprehensive Plan (C/P)															L9
c. Develop & Adopt Incentive-Based Creative Sign Ordinance/Program with the Old Town Yucca Valley Community															L9
d. Continue Refining Regulatory System to Encourage Strategic Development															L9
II. IMPROVE INFRASTRUCTURE															
a. Incorporate Old Town Yucca Valley Streetscape Furniture Palette															F1, F2, S1, L1, L3, L9, L14, L20, L21, L23
b. Design and Build Main Street (Old SR 62) Improvements through Old Town Yucca Valley Core															F1, F2, S1, S3, L1, L3, L9, L14, L20, L21, L23, L24
c. Improve and Build needed Infrastructure Improvements throughout Old Town Yucca Valley Core															F1, F2, S1, S2, S3, L1, L3, L9, L14, L20, L21, L23, L24
i Sewer															see above
ii Water															see above
iii Stormwater															see above
d. Develop and Implement a Long-Term Maintenance Plan															F2, S2, S3, S4, L1, L3, L9, L14, L20, L21, L23, L24
e. Improve Key Local Street Connections through Old Town Yucca Valley Core															F1, F2, S1, L1, L3, L9, L14, L20, L21, L23
i Main Street & Fox Trail															see above
ii Main Street & Deer Trail															see above
iii Main Street & Cherokee Trail															see above
iv Acoma Trail & SR 62															see above
iv Kickapoo Trail & SR 62															see above
f. Introduce Traffic Calming & Beautification Enhancements throughout Old Town Yucca Valley Core															F1, F2, S1, L1, L3, L9, L14, L20, L21, L23
g. Design and Build New SR 62 Improvements around Old Town Yucca Valley Core															F1, F2, S1, S3, L1, L3, L9, L14, L20, L21, L22, L23
III. ENHANCE ENVIRONMENT FOR PEDESTRIANS, BICYCLISTS, & TRANSIT															
a. Design & Install Missing Sidewalk Segments Through Old Town Yucca Valley															F1, F2, S1, S3, L1, L3, L5, L9, L14, L20
b. Create New Pedestrian Connections															F1, F2, S1, S3, L1, L3, L5, L9, L14, L20
c. Accommodate On-Street Bicycle Elements															F1, F2, S1, S3, L1, L3, L5, L9, L14, L20
d. Design and Build Off-Road Trails and Paths															F1, F2, S1, S3, L1, L3, L5, L9, L14, L20
e. Facilitate Public Transit Through New Development and Design Elements															F1, F2, S1, S3, L1, L3, L5, L9, L14, L20

Administration and Implementation

Recommended Programs & Public Improvements/Implementation Matrix

Steps	Actions	Year(s) of Implementation					Implementation Leaders and Team Members								Potential Funding Resources	
		Year 1	Year 2-3	Year 4-6	Year 7-10	Year 11+	TOWN	ST/CA	CNTY	HDWD	MBTA	CoC	OTMA	BWF		Other
URBAN PLANNING & DESIGN RECOMMENDATIONS - Cont.																
IV. PROVIDE CONVENIENT PARKING																
a.	Re-Evaluate Parking Regulations & System															L1, L5, L8, L9, L14
i.	Conduct a Comprehensive Parking Management Study for the Old Town Yucca Valley Village Core															see above
ii.	Refine Parking Space Standards for Land Uses in the Old Town Yucca Valley Village Core															see above
b.	Introduce New Parking in the Old Town Yucca Valley Village Core															F2, L1, L3, L5, L8, L9, L14
i.	Main Street- On Street Diagonal Parking															see above
ii.	Alleys - On Street Parallel Parking															see above
iii.	Small Parking Lots Behind Businesses															see above
c.	Pursue Additional Parking-Related Strategies															F2, L1, L3, L5, L8, L9, L14
i.	Develop & Install a Public Parking Signage Program Throughout Old Town Yucca Valley															see above
ii.	Improve Secondary Storefronts to Facilitate Parking Convenience															see above
V. SUPPORT "COMMUNITY" THROUGH FAMILY-ORIENTED USES & HOUSING																
a.	Develop a Youth and Senior Activity Center in Old Town Core															L1, L3, L7, L9, L20
b.	Develop A Multi-Agency Affordable Housing Solutions Team For Optimizing Housing Choices In Old Town Yucca Valley															L2, L9, L13, L20
c.	Develop Public Plazas and Parks															S2, L1, L3, L7, L9, L20, L25
VI. ENHANCE & MAINTAIN THE OVERALL IMAGE OF OLD TOWN YUCCA VALLEY																
a.	Encourage Adaptive Re-Use & Historic Preservation															S2, S5, S6, L3, L10, L11, L20
i.	Adopt Uniform Code for Building Conservation															see above
ii.	Promote Rehabilitation Loan Programs															see above
b.	Ensure that Infill Development is Compatible with Desired Character															S5, L3, L20
c.	Enhance Existing Building Facades															S5, L1, L3, L9, L10, L11, L19, L20
i.	Develop Façade Improvement Program															see above
ii.	Locate Funding for Program Incentive															see above
c.	Utilize Landscaping to Promote Image and Comfort															S2, L5, L9, L14, L17, L20
i.	Introduce Color Through Plantings															see above
ii.	Introduce Comfort and Reinforce Street Edges Through Trees and Shrubs															see above
iii.	Use a Plant Palette Tailored for Old Town Yucca Valley															see above
d.	Introduce Decorative Paving to Reinforce Safety															S2, L5, L9, L14, L17, L20
e.	Use Public Art to Add Visual Interest															L16, L9
f.	Protect Views to the Mountains															L9
g.	Build Gateways that Welcome People to Old Town Yucca Valley															F1, L1, L5, L9, L20
i.	Main Street & SR 62															see above
ii.	Main Street & Yucca Trail (New SR 62)															see above
h.	Install Wayfinding and Identity Signage System															F1, L1, L4, L5, L9, L17, L28, L20
i.	Size, Locate, and Color Vehicular Wayfinding Signs Appropriately															see above
ii.	Design Pedestrian Wayfinding Signs Appropriately															see above
iii.	Unify the City & Reinforce Village Identity Through Banners															see above
iv.	Introduce Simple Directional Signs to Businesses and Districts															see above
i.	Plan for Long-Term Maintenance															S4, L5, L6
j.	Organize a Old Town Yucca Valley Cleanup Days and a Good "SAM" (Safety & Maintenance Program)															F1, L5, L6, L9
VII. CREATE AN OLD TOWN CENTER																
a.	Acquire Parcels Near Elk Trail, North of Main Street															L1, L3, L5, L7, L9, L20
b.	Encourage Town Public Amenities to be Moved or a Branch Located within new Town Center															L3, L5, L7, L9, L15, L20
i.	Library															see above
ii.	Visitor Center															see above
c.	Develop a Town Plaza															S2, S3, L1, L3, L5, L7, L9, L15, L20, L25
i.	Main Street & Elk Trail															see above
e.	Coordinate with the U.S. Postal Service to Locate a Branch Post Office in Old Town Yucca Valley															L9

Recommended Programs & Public Improvements/Implementation Matrix

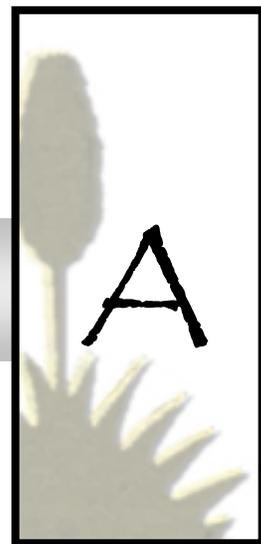
Steps Actions	Year(s) of Implementation					Implementation Leaders and Team Members								Potential Funding Resources	
	Year 1	Year 2-3	Year 4-6	Year 7-10	Year 11+	TOWN	ST/CA	CNTY	HDWD	MBTA	CoC	OTMA	BWF		Other
TOWNWIDE ECONOMIC DEVELOPMENT RECOMMENDATIONS															
I. STABILIZE LOCAL COMMERCE															
a. Build a Business Advisory Team															na
b. Identify and Assist Anchor Businesses															F1, F2, S5
i Work with Partners to Enhance Business Development Resources															see above
ii Refine Business Assistance Delivery System															see above
iii Expand Outreach to Anchor Businesses															see above
c. Tap and Direct Resources															na
d. Enhance Services Delivery to Tourism Target Markets															F2, L4, L5, L9
i Define Primary Tourism Target Markets															see above
ii Obtain Better Research on Existing Tourism Markets															see above
iii Develop Tourism Assets in Context of Strategic Target Markets															see above
iv Refine Promotional Programs to Reach Target Markets Better															see above
v Adopt Tourism Development System Based on Three Key Goals:															see above
1. Expand Average Length of Stay															see above
2. Increase Average Dollars Spent Per Day															see above
3. Encourage Additional Tourism in Weak Seasons Only															see above
II. DIVERSIFY THE ECONOMY															
a. Build New Industries From Existing Strengths & Assets															F2
i Investigate Products That Can Be Sold to Existing Visitors															see above
ii Recruit Small Businesses Within Capacity to Serve															see above
b. Encourage Culture & Arts-based Business Development															F2
c. Improve Business Services to Visitors: Satellite Office Services															F2
d. Create Business Incubation Feasibility Study and Development Plan															F2
e. Enhance Continuing Education and Workforce Training Options.															F2

Administration and Implementation

Recommended Programs & Public Improvements/Implementation Matrix

Steps Actions	Year(s) of Implementation					Implementation Leaders and Team Members								Potential Funding Resources	
	Year 1	Year 2-3	Year 4-6	Year 7-10	Year 11+	TOWN	ST/CA	CNTY	HDWD	MBTA	CoC	OTMA	BWF		Other
OLD TOWN YUCCA VALLEY'S BUSINESS DEVELOPMENT RECOMMENDATIONS															
I. DEFINE OLD TOWN YUCCA VALLEY'S MARKETS															
a. Research Old Town Yucca Valley's Current Customers															F2
b. Define Primary, Secondary, Tertiary Markets															F2
c. Communicate Research Results to Businesses															F2
II. CLARIFY OLD TOWN YUCCA VALLEY'S NICHE & IDEAL BUSINESS MIX															
a. Develop a Old Town Yucca Valley District Mission Statement															na
b. Refine Old Town Yucca Valley's Niche and Image Package															na
c. Identify Old Town Yucca Valley's Ideal Business Mix															na
III. PROVIDE SUPPORT FOR BUSINESS RETENTION & EXPANSION															
a. Develop Products/Services for Market Needs															F2, L4
b. Business Training															F2, L4
c. Low-interest Loan Programs															L1, L4
IV. RECRUIT STRATEGIC BUSINESSES															
a. Identify Available Commercial Space in Old Town Yucca Valley															F2
b. Create Business Recruitment Packet															F2
c. Target Strategic Businesses															F2
V. ENHANCE TOURISM SERVICE & PRODUCTS															
a. Initiate Strategic Sign Program															F1, L4, L18
b. Inventory and Promote Meeting Facilities															L4
c. Make Old Town Yucca Valley's History Visible															S2, L4, L17, L18
ii Enact Interpretive Sign program															see above
d. Initiate Customer Service Training															F2
VI. REFINE SYSTEM OF FESTIVALS & EVENTS															
a. Evaluate Current Events															F2, L4
b. Expand/Reschedule/Identify New Events															F2, L4
c. Develop New Off-Peak Season Events															F2, L4
VII. INITIATE STRATEGIC PROMOTIONS															
a. Identify Collaborative Opportunities/Resources															na
b. Identify Goals; Prioritize Markets															na
c. Select Themes															na
d. Develop Strategic Program & Budget															na
e. Select Media, Methods, Follow-up Strategy															na
f. Implement															F2, L4
g. Track & Evaluate															na

APPENDIX A



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APPENDIX - A

Contact List

Town of Yucca:

Monterey Business Center
58928 Business Center Drive
Yucca, CA

Phone: (760)369-6575

Contact: Shane Stueckle, Deputy Town Manager

RBF Consulting:

14725 Alton Parkway
Irvine, CA 92618

Phone: (949)855-3417

Contact: Al Zelinka, Project Manager

Planning/Engineering Team

Susan Jackson Harden, AICP - Planning

David Barquist, AICP - Planning

Jennifer Gates- Planning

Kimberly Ruddins, AICP - Planning

Bob Matson, P.E. - Transportation

Charlie Marr, P.E. - Water Utilities

Economic –Stanley Hoffman Associates:

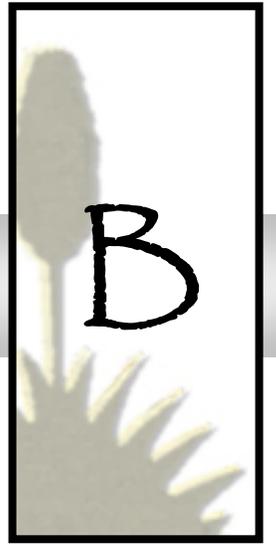
11661 San Vicente Boulevard, Suite 306
Los Angeles, CA 90049

Phone: (310)820-2680

Contact: Stanley Hoffman

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APPENDIX B



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APPENDIX - B

Glossary of Terms

Alley: A public way permanently reserved as a secondary means of access to abutting property

Ancillary use: A use which is incidental or supplementary to a primary permitted use.

Area, gross: A unit of land measure, including easements, existing and future rights-of-way and other future dedications.

Area, net: A unit of land measure, not including the area within the established rights-of-way of a public or private street, or any other area dedicated or required to be dedicated in the future for a public use.

Conditional Use Permit (CUP): A zoning instrument used primarily to review the location, site development, or operation of certain land uses. A conditional use permit is granted at the discretion of the Planning Commission or Zoning Administrator and is not the automatic right of the applicant or landowner.

Conforming Uses Permitted: Those uses identified to incorporate drive thru facilities, which comply with identified performance standards to the satisfaction of the City. Proposed uses that satisfy the required performance standards shall be allowed to apply for a "Conforming Use Permit" that shall be approved by the City Planning Director and shall be kept on file with the City of Ontario.

Development Advisory Board (DAB): A board in the City as established by the City Council charged with the responsibility for the review and approval of development plans.

Dwelling unit, single family: An attached or detached building not to contain more than one kitchen and which, regardless of the form of ownership, is not designed to accommodate more than one household.

Dwelling unit, multi-family: One or more rooms designed, occupied, or intended for occupancy as separate living quarters, with cooking, sleeping and sanitary facilities provided within the same unit for the exclusive use of the household.

Floor area ratio (FAR): The total building square footage on a given lot, divided by the lot area of the same lot. Building square footage includes all structures on the lot, including garages and accessory structures.

Height, building: The vertical dimension of a building or any other structure, measured from the highest point of the roof to adjacent grade within five feet of the building immediately below the point of measurement, not including chimneys, antennas, elevators or other appurtenant structures.

Home occupation: An occupation conducted by the occupant of a dwelling as a secondary use in which there is no display, no stock-in-trade, no commodity sold on the premises, mechanical equipment used except for that necessary for housekeeping purposes.

Live-Work: A dwelling unit that acts as both a residence and a place of commercial activity, where the residential use is the primary use, and the commercial activity is the secondary use.

Open space: Any parcel or area of land or water essentially unimproved and set aside, dedicated, designated, or reserved for public or private use.

Setback, front yard: The horizontal distance between the front property line and a line parallel thereto at the nearest point of a structure on the site.

Setback, rear yard: The horizontal distance between nearest part of a main building and the nearest point of the rear property line.

Setback, side yard: The horizontal distance between the side property line and a line parallel thereto at the nearest point of a structure on the site.

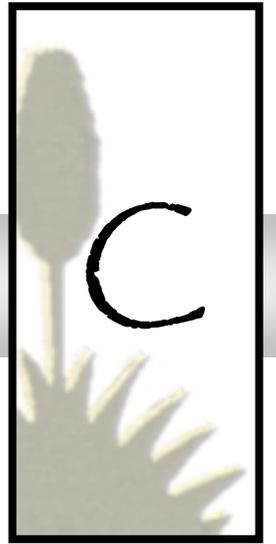
Street, arterial: A street with signals at important intersections and stop signs on the side streets, that collects and distributes traffic to and from collector streets.

Street, collector: A street that collects traffic from local streets and connects with arterial streets.

Street, local: A street designed to provide vehicular access to abutting property and to discourage through traffic.

Use: The purposes for which a site or a structure is arranged, designed, intended, constructed, or erected.

APPENDIX C



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APPENDIX - C

GENERAL PLAN CONSISTENCY

Consistency with the Yucca Valley General Plan

California law requires a Specific Plan be consistent with the General Plan of the adopting locality. To this end, existing General Plan goals and policies were reviewed to ensure consistency between the Old Town Yucca Valley Specific Plan and the Yucca Valley General Plan (1995). The Yucca Valley General Plan provides a supportive foundation for the Specific Plan and reinforces the goals and policies for the Old Town area, as evident in the highlighted General Plan goals and policies that follow.

◆ Land Use Element

The following land use goals support the Old Town Yucca Valley Specific Plan:

- Goal- A balanced mix of functionally integrated land uses which meet general social and economic needs of the community through compatible and harmonious land use and zoning designations.
- Goal- A well rounded community of desirable neighborhoods, a strong employment base and a variety of community facilities.

The following land use policies support the Old Town Yucca Valley Specific Plan:

Policy (Commercial)- Enhance the character and viability of commercial areas by providing an integrated mix of commercial areas by providing an integrated mix of commercial, office and residential uses.

Policy (Residential)- Encourage in-fill development on subdivided lands located adjacent to existing residential areas.

Policy (Industrial)- Encourage redevelopment of existing older or marginal industrial lands.

Response:

The Specific Plan aims to promote new compatible development in Old Town Yucca Valley that integrates a variety of compatible land uses including commercial, residential, and light industrial. It seeks to allow a mix of uses including residential within the core area. The development regulations and design guidelines contained within the Specific Plan reinforce the policies of the General Plan, which promote quality design, compatible in-fill development, and a mix of uses.

◆ Circulation Element

The following circulation goal supports the Old Town Yucca Valley Specific Plan:

- Goal- A circulation network that efficiently, safely and economically moves people, vehicles and goods using transportation facilities that meet the current demands and

projected needs of the Town, while maintaining and protecting its rural residential character.

The following circulation policies support the Old Town Yucca Valley Specific Plan:

Policy- Coordinate and cooperate with CalTrans to assure preservation of capacity and maximized efficiency along State Highway 62.

Policy- Create new alternative, east/west arterials and smaller capacity routes to enhance intra-town circulation and relieve congestion on Highway 62.

Policy- Encourage expansion of ridership and the mass transit systems operated within the Town and greater Morongo Basin.

Policy- Facilitate the design and installation of a community locations/directional signage program to efficiently direct traffic.

Response:

The Specific Plan sets regulations and design guidelines that are compatible with different circulation systems within Old Town Yucca Valley creating a pedestrian oriented environment along the major commercial corridor. To maximize the efficiency of SR-62 and enhance intra-town circulation, new route alternatives were taken into account when developing the Specific Plan. Gateway and wayfinding signage was sited throughout Old Town to increase awareness and guide visitors and residents.

◆ Housing Element

The following housing goals support the Old Town Yucca Valley Specific Plan:

o Goal- The development of a variety of housing types and prices in the Town of Yucca Valley that will accommodate both existing and future residents within all socio-economic segments of the community.

o Goal- The development of affordable housing projects in the Town of Yucca Valley for very low, low and moderate income households to meet the community's need.

o Goal- The maintenance and rehabilitation of the Town's core neighborhoods.

The following housing policies support the Old Town Yucca Valley Specific Plan:

Policy- Promote and facilitate the use of State and Federal monies for the development and rehabilitation of affordable housing in the community.

Policy- Promote and preserve mobile home parks for their value as low and moderate income housing opportunities.

Policy- Encourage the preservation of hometown and rural atmosphere through design standards.

Response:

The Specific Plan allows for new market-rate and affordable housing opportunities within the downtown through new mixed-use and live-work development. The development regulations and design guidelines ensure that the development is compatible with the rural environment.

◆ Parks, Recreation, and Trails Element

The following parks, recreation, and trails goals support the Old Town Yucca Valley Specific Plan:

- o Goal- A multi-use, quality system of parks, and recreational areas that support a broad range of activities, as well as cultural, and passive open space enjoyment opportunities for current and future residents.
- o Goal- An enhanced and expanded park and recreational system designed to provide opportunities for healthful active, passive and cultural enjoyment throughout the Town and to all segments of the population.
- o Goal- An optimally utilized community system of multi-use equestrian, bicycling, and pedestrian trails, with rest areas and other amenities, which conveniently link existing and proposed open space and recreational areas within the Town of Yucca Valley.

The following parks, recreation, and trails policies support the Old Town Yucca Valley Specific Plan:

Policy- Parklands development promoting water conservation and the preservation of the Town's natural resources.

Review new development for their potential to incorporate appropriate pedestrians, equestrian, and bicycle trail connections to the Town-wide recreational system.

Response:

The Specific Plan integrates parks and trails within Old Town that create opportunities for resident and visitor enjoyment while incorporating compatible desert landscaping. The new trails are linked to the Town-wide system.

◆ Community Design Element

The following community design goal supports the Old Town Yucca Valley Specific Plan:

- o Goal- An integrated, coherent and flexible set of principles that direct community judgment on land use, environmental, ecological, economic, aesthetic, and spiritual values and aspirations of the Town of Yucca Valley.

The following community design policies support the Old Town Yucca Valley Specific Plan:

Policy- Design standards consistent with its character as a low density, rural residential community that also provides regional commercial and professional services.

Policy- Design parameters for future development which protect the community's scenic viewsheds, provide community cohesion, and enhance the image.

Policy- Use specific plans to implement the "Mixed Use" land use designation.

Response:

The Specific Plan contains development regulations and design guidelines that promote quality design, ensure compatible development, and protect the viewsheds and character of Old Town. Through public involvement, the vision for Old Town was developed, incorporating the values of the residents and stakeholders of the Town.

◆ Economic Development Element

The following economic development goals support the Old Town Yucca Valley Specific Plan:

- o Goal - A broadly-based, healthy and balanced economy that provides a full range of economic and employment opportunities.
- o Goal - Continued growth which assures the maintenance of a revenue base adequate to support present and future public services and facilities needs.

The following economic development policies support the Old Town Yucca Valley Specific Plan:

Policy- Strengthen Town's role as a commercial center.

Policy- Encourage the development of the tourist/traveler commercial potential of the State Highway 62 corridor.

Policy- Encourage and enhance quality development and renovation in the downtown area, along Highway 62.

Response:

The Specific Plan provides opportunities for a mix of uses and retail types in Old Town with districts that provide both visitors and residents different commercial, office, and housing options. The design guidelines promote high quality design and improvements to rear facades.

◆ Public Services Element

The following public services goals support the Old Town Yucca Valley Specific Plan:

- o Goal- To assure the compatible and aesthetic integration of public buildings and facilities in order to provide existing and future residents with adequate public services.
- o Goal- A full range of public utilities and related services in the Town of Yucca Valley that provide for the immediate and long term needs of the entire community.

The following public services policies support the Old Town Yucca Valley Specific Plan:

Policy- Encourage the development of public services and facilities in a manner that assures adequate levels of service, while remaining compatible with existing and future land uses.

Policy- Develop public buildings and facilities, which house Town government activities in a functional, aesthetically pleasing, and convenient place for residents and Town officials to conduct business. Public Art should be encouraged in the development of public facilities.

Policy- Confer and cooperate with the Hi-Desert Water District to assure an adequate water system for existing and future development and maintain an adequate reserve of water in storage facilities.

Response:

The Specific Plan supports the public services policies within the General Plan. A water study was completed and suggestions were made to meet future demand. The Vision Plan depicts a new Town Square in Old Town where new civic and public facilities could be located as the need for new facilities arises.

◆ Environmental Resources Element

The following environmental resources goals support the Old Town Yucca Valley Specific Plan:

- o Goal- Conservation, management, and designation of open space areas to protect environmental resources, guard against environmental hazards, and provide enhanced recreational opportunities and aesthetic character for the Town.
- o Goal (Biological Resources)- Protect and preserve the Town's biological resources and a functional, harmonious relationship and balance between nature and human development.

- o Goal (Archaeological and Historic Resources)- Preservation, maintenance, continuity and enhancement of cultural heritage and resources in the Town of Yucca Valley.

The following environmental resources policy support the Old Town Yucca Valley Specific Plan:

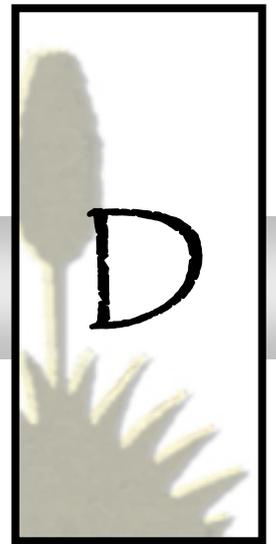
Policy- Identify lands suitable and appropriate for preservation as open spaces.

Response:

The Specific Plan highlights the need for a historic and archaeological survey to identify important local resources. The plan also recommends the use of native, drought tolerant landscaping that is compatible the existing natural environment and conserves water.

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APPENDIX D



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APPENDIX -D

SUPPORT INFORMATION

1. Transportation Memorandum
2. Water Utility Memorandum

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MEMORANDUM

To: Al Zelinka, RBF Consulting JN 10104353
From: Bob Matson, RBF Consulting
Date: May 24, 2005
Subject: **Proposed Twentynine Palms Highway Bypass Traffic Assessment**

As you requested, RBF has prepared an assessment to evaluate the traffic impacts associated with constructing a proposed State Route 62 (SR-62) Bypass in the Town of Yucca Valley between Kickapoo Trail and Apache Trail. Twentynine Palms Highway, designated SR-62, provides east-west access for the Town of Yucca Valley and the Morongo Basin.

The proposed Yucca Valley Downtown Specific Plan project includes a proposal to construct a SR-62 Bypass to allow east-west traffic to travel around the Town without impacting the downtown Yucca Valley area. Just east of Kickapoo Trail, the proposed SR-62 Bypass would trend northerly to align with Yucca Trail north of the existing Twentynine Palms Highway. The SR-62 Bypass would rejoin the existing Twentynine Palms Highway in the vicinity of Apache Trail.

This assessment evaluates forecast demand, capacity, and level of service of the following three study roadway segments without and with the proposed SR-62 Bypass assuming buildout of the Town of Yucca Valley General Plan:

- Yucca Trail;
- Twentynine Palms Highway; and
- Santa Fe Trail.

Analysis Methodology

Level of service (LOS) is commonly used as a qualitative description of roadway operation and is based on the capacity of the roadway segment and the volume of traffic using the roadway segment. Table 1 shows the Town of Yucca Valley roadway capacity of average daily traffic (ADT) based on roadway classification and LOS E.

**Table 1
Town of Yucca Valley Roadway Capacity LOS & V/C Ranges**

Number of Lanes/ Roadway Description	Average Daily Traffic (based on LOS E)
8 / raised median	75,000
6 / raised median	56,300
4 / raised median	37,500
4 / no median	25,000
2 / painted median	18,800
2 / no median	12,500

Source: Town of Yucca Valley General Plan Circulation Element

Note: Applicable criteria used in this analysis are shown in bold.

The V/C ratio analysis method is utilized by the Town of Yucca Valley to determine the operating LOS of the study roadways, which describes the operation of a roadway segment using a range of LOS from LOS A (free-flow conditions) to LOS F (severely congested conditions), based on corresponding V/C ratios as shown in Table 2.

**Table 2
LOS & V/C Ranges
Roadway Segment**

LOS	V/C Ratio
A	≤ 0.60
B	0.61 to ≤ 0.70
C	0.71 to ≤ 0.80
D	0.81 to ≤ 0.90
E	0.91 to ≤ 1.00
F	> 1.00

Performance Criteria

The Town of Yucca Valley goal for roadway segment operation is LOS D or better.

Study Roadway Classification

According to the Town of Yucca Valley General Plan Circulation Element, Twentynine Palms Highway (SR-62) is classified as a six-lane divided highway, Yucca Trail is classified as a two-lane divided industrial roadway, and Santa Fe Trail is classified as a four-lane divided collector. Exhibit 1 shows the General Plan Circulation Element classification of the study roadways.

Forecast General Plan Buildout Conditions Daily Traffic Volumes

This analysis is based upon forecast traffic volumes contained in the *Town of Yucca Valley General Plan Program Circulation Element Traffic Study (Robert Kahn, John Kain and Associates, August 24,*

1995), which assumes buildout of the Town of Yucca Valley General Plan. Exhibit 2 shows the forecast General Plan buildout conditions traffic volumes at the study roadway segments.

Forecast General Plan Buildout Conditions Roadway Segment Level of Service

Exhibit 3 and Table 3 summarize the forecast General Plan buildout conditions V/C ratio and corresponding LOS of the study roadway segments.

**Table 3
Forecast General Plan Buildout Conditions Roadway Segment LOS**

Roadway Segment	Classification	Volume	Capacity	V/C Ratio	LOS
Yucca Trail	2-lane Divided Industrial	5,000	18,800	0.27	A
Twentynine Palms Highway (SR-62)	6-lane Divided Highway	48,000	56,300	0.85	D
Santa Fe Trail	4-lane Divided Collector	6,000	37,500	0.16	A

As shown in Table 3, all study roadway segments are forecast to operate at an acceptable LOS (LOS D or better) according to Town of Yucca Valley performance criteria for forecast General Plan buildout conditions.

Proposed SR-62 Bypass

With implementation of the proposed realignment of SR-62, the existing Twentynine Palms Highway between Kickapoo Trail and Apache Trail would change classification to a two-lane divided industrial roadway and no longer be designated SR-62, while Yucca Trail would be classified as a six-lane divided highway and designated SR-62; Santa Fe Trail would remain classified as a four-lane divided collector.

Forecast General Plan Buildout With SR-62 Bypass Conditions Daily Traffic Volumes

Forecast General Plan buildout with proposed SR-62 Bypass conditions traffic volumes were derived by reassigning ADT to the study area roadway network assuming the proposed SR-62 bypass. Exhibit 4 shows the forecast General Plan buildout with proposed SR-62 Bypass conditions traffic volumes at the study roadway segments.

Forecast General Plan Buildout With SR-62 Bypass Conditions Roadway Segment LOS

Exhibit 5 and Table 4 summarize the forecast General Plan buildout with proposed SR-62 Bypass conditions V/C ratio and corresponding LOS of the study roadway segments.

**Table 4
Forecast General Plan Buildout With SR-62 Bypass Conditions Roadway Segment LOS**

Roadway Segment	Classification	Volume	Capacity	V/C Ratio	LOS
Yucca Trail (SR-62)	6-lane Divided Highway	44,500	56,300	0.79	C
Twentynine Palms Highway	2-lane Divided Industrial	7,500	18,800	0.40	A
Santa Fe Trail	4-lane Divided Collector	7,000	37,500	0.19	A

As shown in Table 4, all study roadway segments are forecast to operate at an acceptable LOS (LOS D or better) according to Town of Yucca Valley performance criteria for forecast General Plan buildout with proposed SR-62 Bypass conditions.

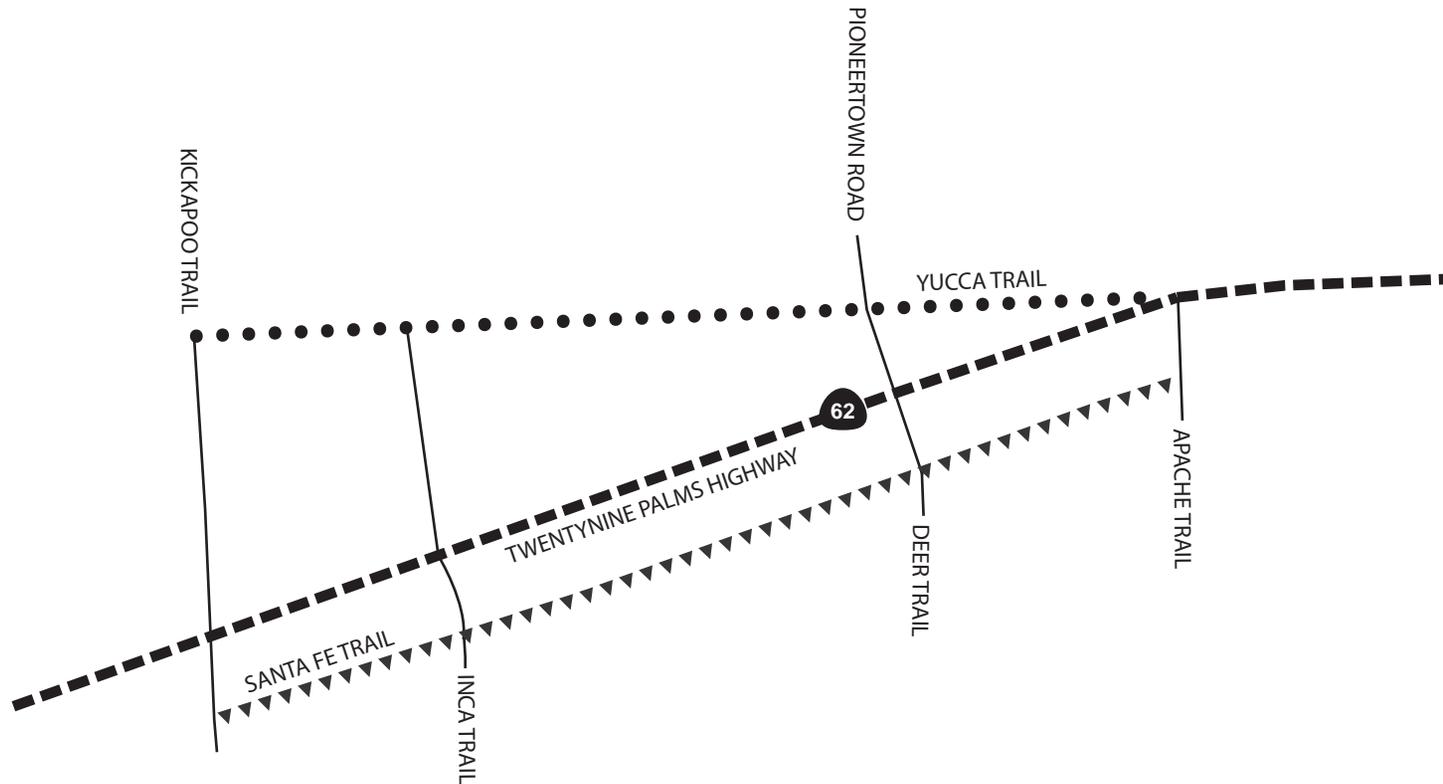
Conclusions

The study area roadway network is forecast to operate at an acceptable LOS (LOS D or better) at Town of Yucca Valley General Plan buildout conditions without and with the proposed SR-62 Bypass project.

The V/C ratio of SR-62 is forecast to improve to 0.79-LOS C with the proposed SR-62 Bypass project from 0.85-LOS D without the proposed SR-62 Bypass project.

The V/C ratio and LOS of Twentynine Palms Highway is forecast to improve to 0.40-LOS A with the proposed SR-62 Bypass project from 0.85-LOS D without the proposed SR-62 Bypass project.

H:\pdata\10104353\Traffic\Admin\4353_Capacity Memo.doc



Legend:

- ▬▬▬▬ Highway - 6 lanes divided
- ▼▼▼ Major Collector - 4 lanes divided
- Industrial - 2 lanes divided

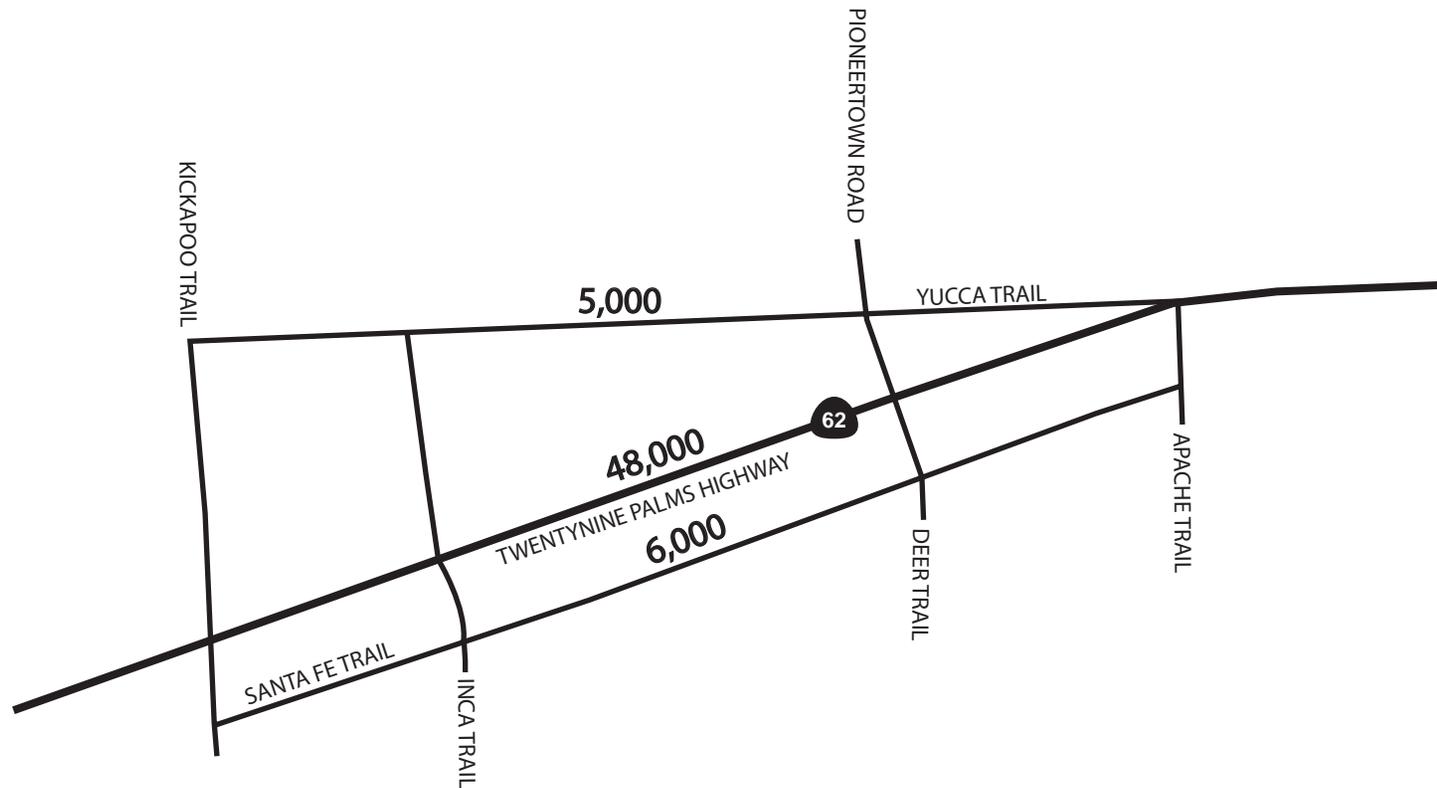


Not to Scale



Source: Town of Yucca Valley General Plan Circulation Element

Study Roadway Segments Functional Classification



Legend:

X,XXX Average Daily Traffic

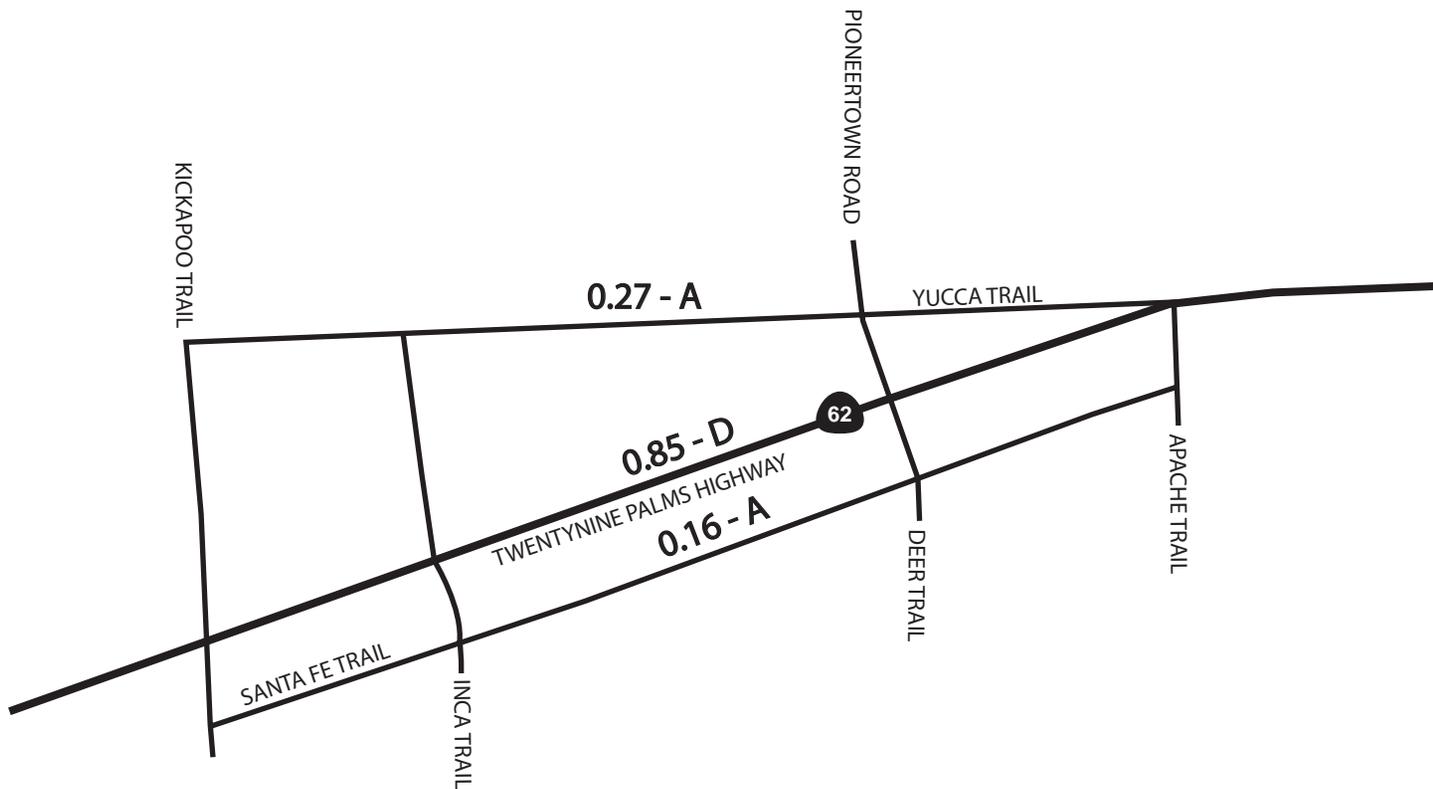


Not to Scale



Source: Town of Yucca Valley General Plan
 Program Circulation Element Traffic Study
 (Robert Kahn, John Kain, and Associates, August 24, 1995)

Forecast General Plan Buildout ADT Volumes



Legend:

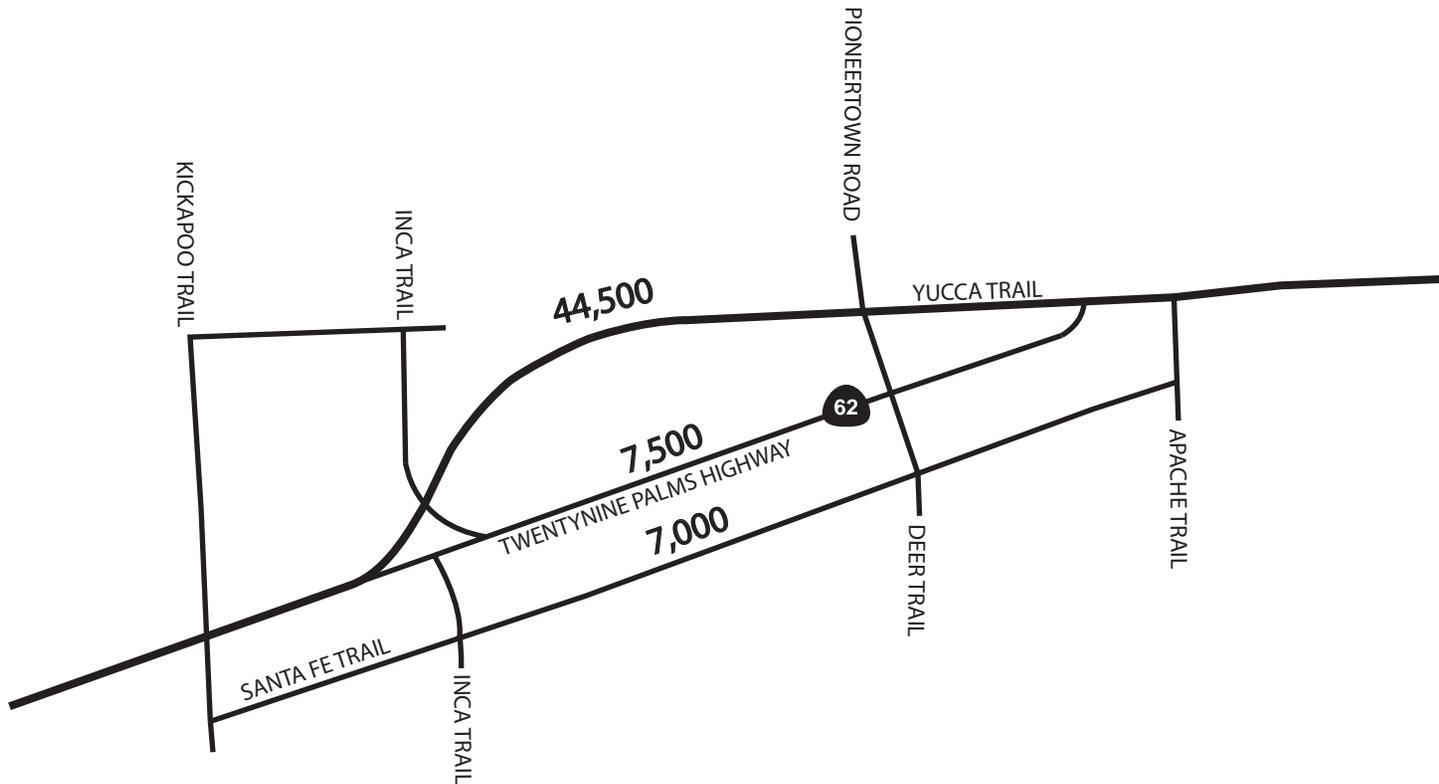
X.XX-X Volume/Capacity Ratio - Level of Service



Not to Scale



Forecast General Plan Buildout Conditions Roadway Segment V/C Ratio & LOS



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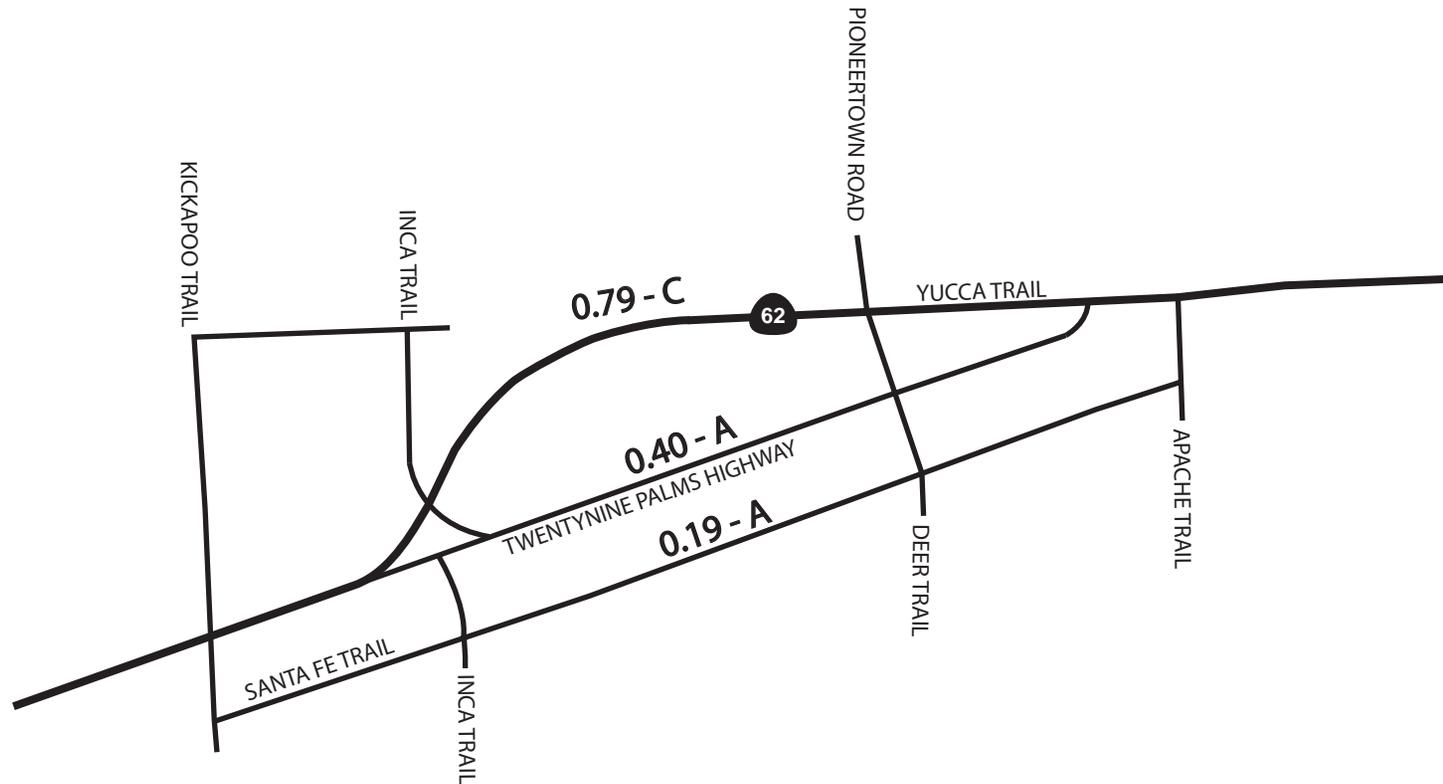
X,XXX Average Daily Traffic



Not to Scale



Forecast General Plan Buildout With SR-62 Bypass Project ADT Volumes



Legend:

X.XX-X Volume/Capacity Ratio - Level of service



Not to Scale



Forecast General Plan Buildout With SR-62 Bypass Project Roadway Segment V/C Ratio & LOS

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MEMORANDUM

To: Al Zelinka & David Barquist, MS 445 JN 10-103453
From: Corey Hess and Charlie Marr, MS 210
Date: September 19, 2005
Subject: Yucca Valley Revitalization Project – Old Town Specific Plan Draft Utility

The Town of Yucca Valley is located north of the desert city of Palm Springs, across the San Bernardino County line. Yucca Valley and surrounding cities and communities are commonly known as the High Desert due to its elevation above mean sea level. The Yucca Valley Revitalization (Project) study area encompasses the Old Town Specific Plan and is located within the Yucca Valley city limits along the SR 62 Highway from Katje Way to approximately 650 lineal feet east of Church Street, as shown in Exhibit 1. The study area is divided into four districts as the Old Town Commercial/Residential; Old Town Highway Commercial; Old Town Mixed Use; and Old Town Industrial. The boundary of each sub-district is shown in Exhibit 2.

The study area consists of approximately 185 gross acres of various land use types. Existing land uses in the project area include residential, commercial and industrial. A summary of the existing land uses is provided as Table 1. It is anticipated that the revitalization of the inner neighborhoods within the Project area will transform the Project site to an area with high-density residential, commercial, and industrial use along State Route 62. At ultimate build out, the Project could include up to 1,115 residential units and 2,900,604 square feet of non-residential building space.

The Hi-Desert Water District (HDWD or District) currently provides water service to the Project area. Wastewater collection and disposal is performed via private septic systems. With the increased development density that could result from the Revitalization Project, we understand that wastewater service will eventually be converted to a new sewerage collection system. The purpose of this memorandum is to evaluate the existing water system, and make recommendations to ensure appropriate water distribution facilities will be in place to support the

Project under current State and local water system operating requirements and within the intentions of the *Hi-Desert Water District Water Master Plan Update* (December 2001).

Water Supply and Quality

Water service to the study area is provided from the Hi-Desert Water District (HDWD) with the exception of the Blue Skies Country Club, the Institute of Mental Physics, and approximately 16 individual domestic users. The HDWD relies upon the Warren Groundwater Basin in the Valley area as its primary source of water. Although the State Water Project (SWP) recharges the basin, septic tank discharges have contaminated some of the groundwater supply with high nitrate levels. According to the HDWD *2001 Water Supply Master Plan Update*, seven wells pump directly into the District's west side which directly benefits Yucca Valley. The 2001 Master Plan also mentioned the intent of the HDWD to design and build a nitrate and air removal treatment facility for the high nitrate wells on the east side of the District, and was expected to be on-line by early 2002. Therefore, this study assumes a total source capacity for the District's west and east sides of 7.2 mgd, which is adequate for projected needs through 2015 (in 2001, actual maximum day water demand was 4.4 mgd).

Future wastewater improvements (elimination of private septic systems, construction of a new wastewater collection, treatment and disposal systems) should improve the water quality and reduce the contamination problem. As indicated in the Yucca Valley General Plan-Water Resources Element, the maximum contaminant level allowed for nitrate is 45 milligrams per liter. Tests performed in 1992 showed nitrate measurements ranging between 2.9 and 24.1 milligrams per liter. However, as of this writing, no recent test results have been provided.

Water Distribution

The Project area is currently served by the Hi-Desert Water District's (HDWD) water distribution system. The HDWD was originally formed as the Yucca Valley Water District in 1962. The development of the distribution system can be traced back to the purchase of several separate private water systems. In 1971, the District officially changed its name to the Hi-Desert County Water District. In 1980, the District formally adopted its current name. In 1990, the distribution

system expanded through the purchase of the Yucca Water Company. Today, the District has approximately 8,400 service connections, and a service area that spans approximately 52 square miles. In addition, the District operates 16 storage tanks, 17 wells, and maintains nearly 300 miles of transmission and distribution pipelines.

Information was gathered to develop an understanding of the current water distribution system within the Project area. The location and size of the existing facilities were determined from the HDWD February 2002 Water System Atlas and through meetings with District staff. The distribution system in the Project area includes pipelines ranging in size from 2.5- to 12- inches in diameter within the 3495 Pressure Zone.

The 3495 Zone is the largest and lowest major zone within the District's system and acts as the supply zone that the west side wells pump directly into. The December 2001 HDWD master plan identifies a need for additional 3495 Zone distribution storage. Evaluation of storage capacity is included in a later section.

Water Demand

A water demand estimate was calculated for existing users within the Project area for comparison with estimated ultimate Project demands. Demand factors for the estimate are based on factors from Table IV-3 of the District's December 2001 water master plan. These factors appear quite low by current-day standards within the waterworks industry, which is acknowledged in the 2001 water master plan and accepted as a permanent and real water use habit for this high desert region. Other desert areas of Southern California, such as Victorville, Palm Springs, the Beaumont/Banning regions and other unincorporated areas of Riverside County, experience much higher unit usage. This is most likely due to higher irrigation demands for major (common-area) landscape regions and fuel modification zones for fire protection. If the Town of Yucca Valley undergoes other similar redevelopment projects, this could initiate a higher water use trend and overall increase in unit water usage within the HDWD.

For the purpose of comparison, the HDWD unit water usage factors and water demand factors adopted by other desert agencies are as follows:

Land Use Type	HDWD Duty Factor			Other Southern California Desert Regions*
	AF/Ac/Yr	gpd/Ac	gpd/DU	
<i>Rural Residential</i>				
1 DU/Ac	0.3	-	-	0.75 – 3.36 AF/Ac/Yr
1 DU/2.5 Ac	0.15	-	-	0.75 – 3.36 AF/Ac/Yr
1 DU/ 5 Ac	0.08	-	-	0.75 – 3.36 AF/Ac/Yr
1 DU/ 10, 20, 40 Ac	0.03	-	-	0.75 – 2.35 AF/Ac/Yr
<i>Residential</i>				
1 DU/Ac	0.3	268	268	700 - 1500 gpd/DU
2 DU/Ac	0.69	616	308	700 - 1000 gpd/DU
2.5 DU/Ac	0.8	715	286	700 - 800 gpd/DU
5 DU/Ac	1.1	983	197	400 - 540 gpd/DU
<i>Multi Family</i>	4.85	4330	216**	300 - 400 gpd/DU
<i>Industrial/ Commercial</i>	0.26	232	-	1500 - 2000 gpd/Ac

* Borrowed from Eastern Municipal WD and Ranch California WD guidelines.

** Assuming 20 DUs/acre.

Using other Southern California desert regions' demand factors shown above, the existing average day demand for the Project area was determined to be 322,500 gallons per day (gpd), as shown in Table 2A. Using HDWD demand factors shown above, the existing average day demand for the Project area is determined to be 76,000 (gpd), as shown in Table 2B. This represents a four-fold difference in the estimated demands. Estimated peaking of the average demands shows even higher demand estimate differences. Average, maximum-day and peak-hour demands estimates based on other Southern California desert regions and HDWD respective peaking factors are summarized as follows:

Water Demand	Other Southern California Desert Regions	HDWD
Average	322,500 gpd	76,000 gpd
Maximum-day	644,900 gpd	133,100 gpd
Peak-hour	896 gpm	185 gpm

As with existing demand estimate of Tables 2A and 2B, projected water demand of the project is shown in Tables 3A and 3B, respectively. Another important consideration of water demand is fire flow requirements. Fire flow requirements are provided in the District's master plan, and outlined in the following section.

Proposed Water System

The two critical operating scenarios are peak normal demand and fire flow demand. To be sure, public water system operating criteria must always consider peak normal demands. The 2001 *Water Master Plan Update* stressed that a critical operating scenario includes normal peak daily demands (peak-hour) during summer weekday afternoons between 1:00 and 5:00 when most District pumps are not operating. However, the focus of this study is on fire flow requirements due to the likelihood that this scenario is the most critical to plan for.

The existing system has been in operation for many decades and much of the system pipelines have not been replaced since original construction. Current-day water system standards probably require higher fire flows than the criteria at the time of HDWD's system construction. Although the HDWD recently completed several miles of pipeline upgrades during the 1995-96 Pipeline Improvement Project, the area will require further pipeline upgrades to meet fire flow standards under typical water system flow velocity criteria. Fire flow criteria (as provided in the 2001 Water Master Plan Update) and appropriate system pipeline diameter are as follows:

Land Use	Minimum Required Fire Flow (gpm)	Minimum Pipe Diameter	Applicability To Project
Low Density Residential	1,500	8-inch	N/A
Residential	2,000	10-inch (Looping 8-inch)	N/A
Commercial. Multi - Family Residential	3,000	12-inch (Looping 10-inch)	Yes
Industrial	4,000	12-inch (Looping 12-inch)	Yes

For the purposes of the water master plan study for the Specific Plan, pipe diameter is offered here based on the typical, and conservative, industry standard of ten feet per second (fps) velocity. This ensures a reasonable unit headloss within the system for maximum ability to provide the fire flows at the minimum residual pressure of 20 psi dictated by the Uniform Fire Code (UFC). Hydraulic analysis should be performed at the design phase of the Project to verify that these pipe diameters work within the operation of the HDWD transmission system as a whole.

The 1995-96 Pipeline Improvement Project resulted in the construction of 22,300 linear feet of new pipeline replacement projects within the District's west side, which will directly benefit the 3495W Zone and the Project area. However, several older (and smaller – 2-inch, 3-inch, 4-inch) pipelines are still in operation, and some still serve fire hydrants, which are sorely insufficient for providing even the lowest of current-day fire flow requirements. The existing system facilities are shown in Exhibit 3.

In 1997, the District began a pipeline replacement program, which aimed to replace old and undersized pipelines within the District. This study assumes the 2002 Water System Atlas incorporates all replacement projects completed to date. The following table includes notable replacement projects recently completed:

Fiscal Year of Construction	Area	Lineal Footage Installed
2000/ 2001	Jemez Trail and Highland Trail between Kickapoo Trail and Inca Trail	1,500
2000/ 2001	Inca Trail and Mariposa Trail between Mariposa and Fox Trait between Yucca Trail and 29 Palms Hwy	2,300
2002/ 2003	Coyote Trail and Apache Trail, north of 29 Palms Hwy.	3,400

The following table is a list of additional pipeline upgrades recommended as part of the Revitalization Project area:

	Proposed Improvement	Approximate Length (ft)
1	Replace Existing 4" Steel Pipe with 8" PVC pipe	1,300
2	Replace Existing 6" Steel/PVC Pipe with 12" PVC pipe	1,685
3	Replace Existing 8" Steel/PVC Pipe with 10" PVC pipe	3,350
4	Replace Existing 8" Steel/PVC Pipe with 12" PVC pipe	7,580
5	Replace Existing 10" Steel/PVC Pipe with 12" PVC pipe	860
6	Replace Existing Unknown Pipe with 12" PVC pipe	700
7	Install New 8" PVC Pipe	3,770
8	Install New 10" PVC Pipe	2,820
9	Install New 12" PVC Pipe	5,655
10	Abandon 2.5" Steel Pipe in R-O-W	160
11	Abandon 3.5" PVC/ Steel Pipe in R-O-W	700
12	Abandon 4" Steel Pipe in R-O-W	2,495
13	Abandon 8" Steel Pipe in R-O-W	1,650
14	Abandon 10" Steel Pipe in R-O-W	905
15	Abandon 12" ACP Pipe in R-O-W	1,150
16	Abandon "Unknown" PVC Pipe in R-O-W	1,840

The Revitalization Project could also represent an opportunity to ensure adequate fire hydrant coverage. In locations that cannot be reached by conventional fire department equipment from existing public fire hydrants, new fire hydrants could be added and/or old hydrants replaced as part of the infrastructure upgrades. Exhibit 4 shows the pipeline upgrades recommended within

the Project area. The proposed system upgrades should be verified with computer model simulation prior to design.

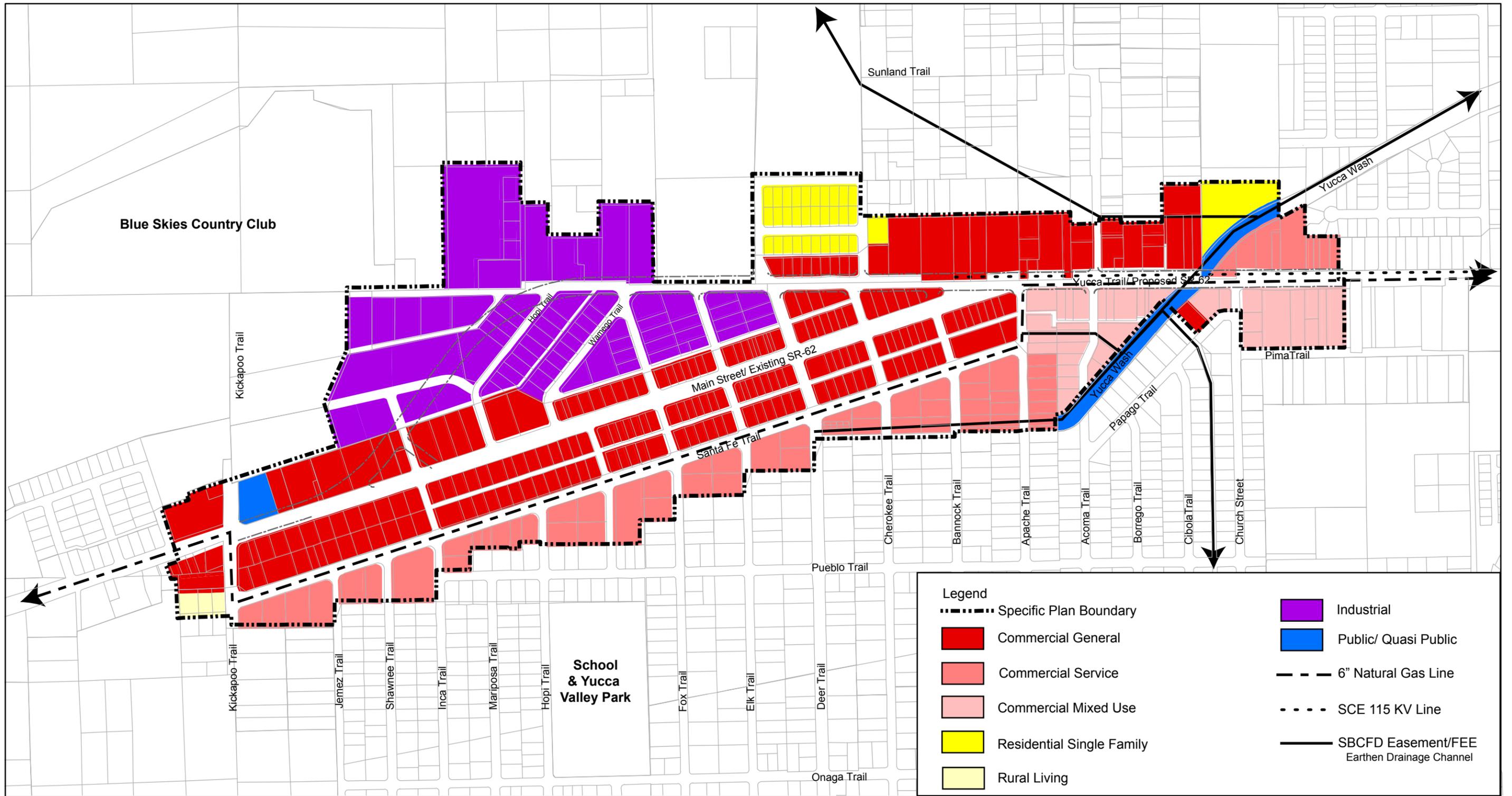
Storage

The master plans define storage requirements due to three separate needs – operational, emergency and fire. Both the 1995 and 2001 master plans discuss the need for additional storage in the 3495 Pressure Zone. The *2001 HDWD Water Master Plan Update* (Section VII) describes additional storage capacity needs to the District's existing (2001) storage capacity of 4.5 million gallons (MG). Projected water demands for the 3495 Pressure Zone (both East and West sides) produce a need for 4.72 million gallons (MG) for Year 2005, and 5.57 MG for Year 2020, according to Tables VII-1B and VII-1A of the Update. This represents an additional storage need for the 3495 Zone, as a whole, of approximately 0.2 MG and 1.1 MG, respectively. A comparison of Tables 2B and 3B estimates the Revitalization Project adding approximately 205,500 gallons of demand during a maximum day. Assuming fire flow capacity already exists in the existing storage capacity, and depending on the additional storage needs attributable to the West side, current storage capacity in the 3495W Zone may be adequate for the additional demands estimated from the Project.

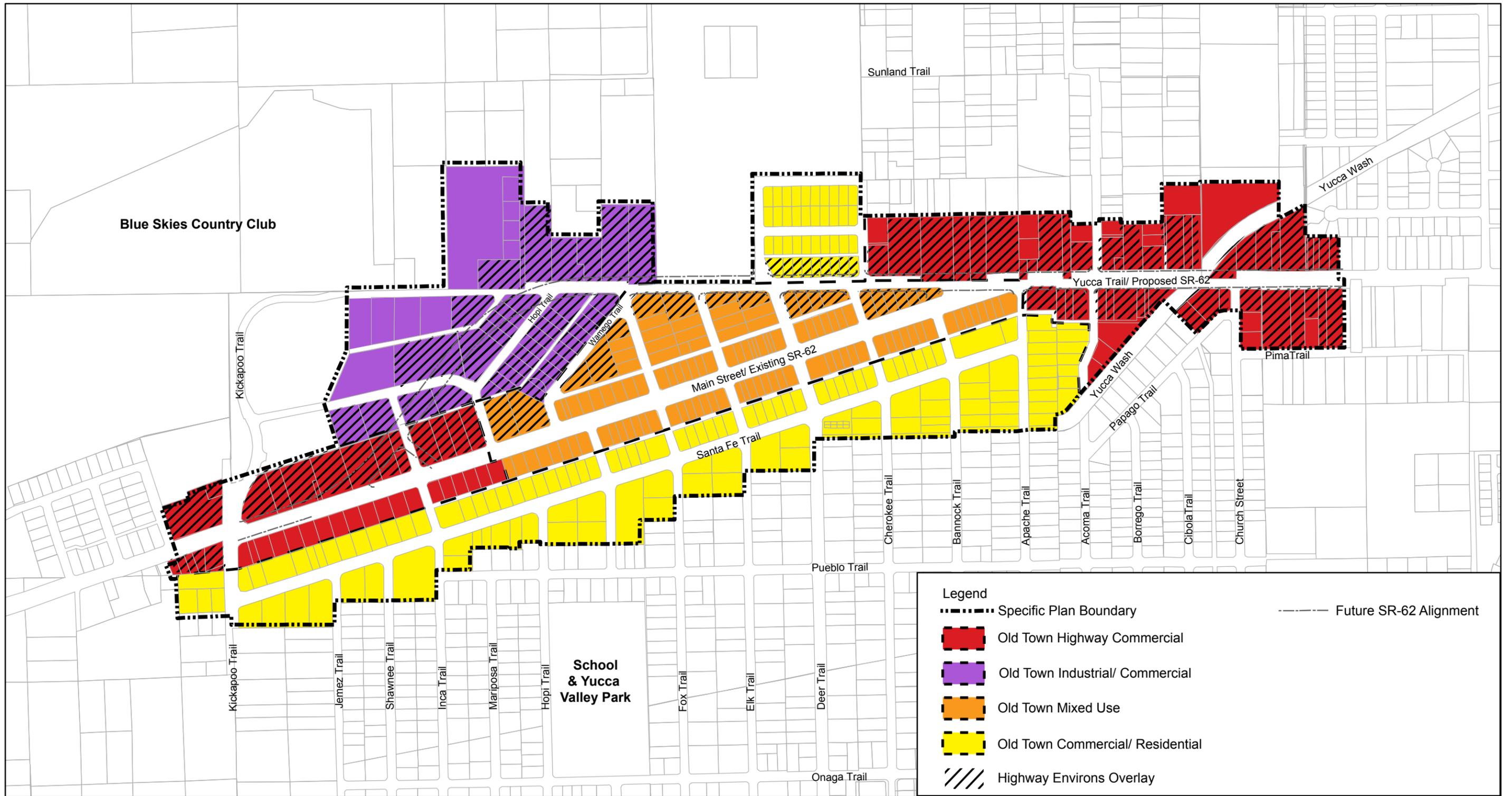
Recommendations

The following recommendations are offered as a result of this water system infrastructure study for the Revitalization Project:

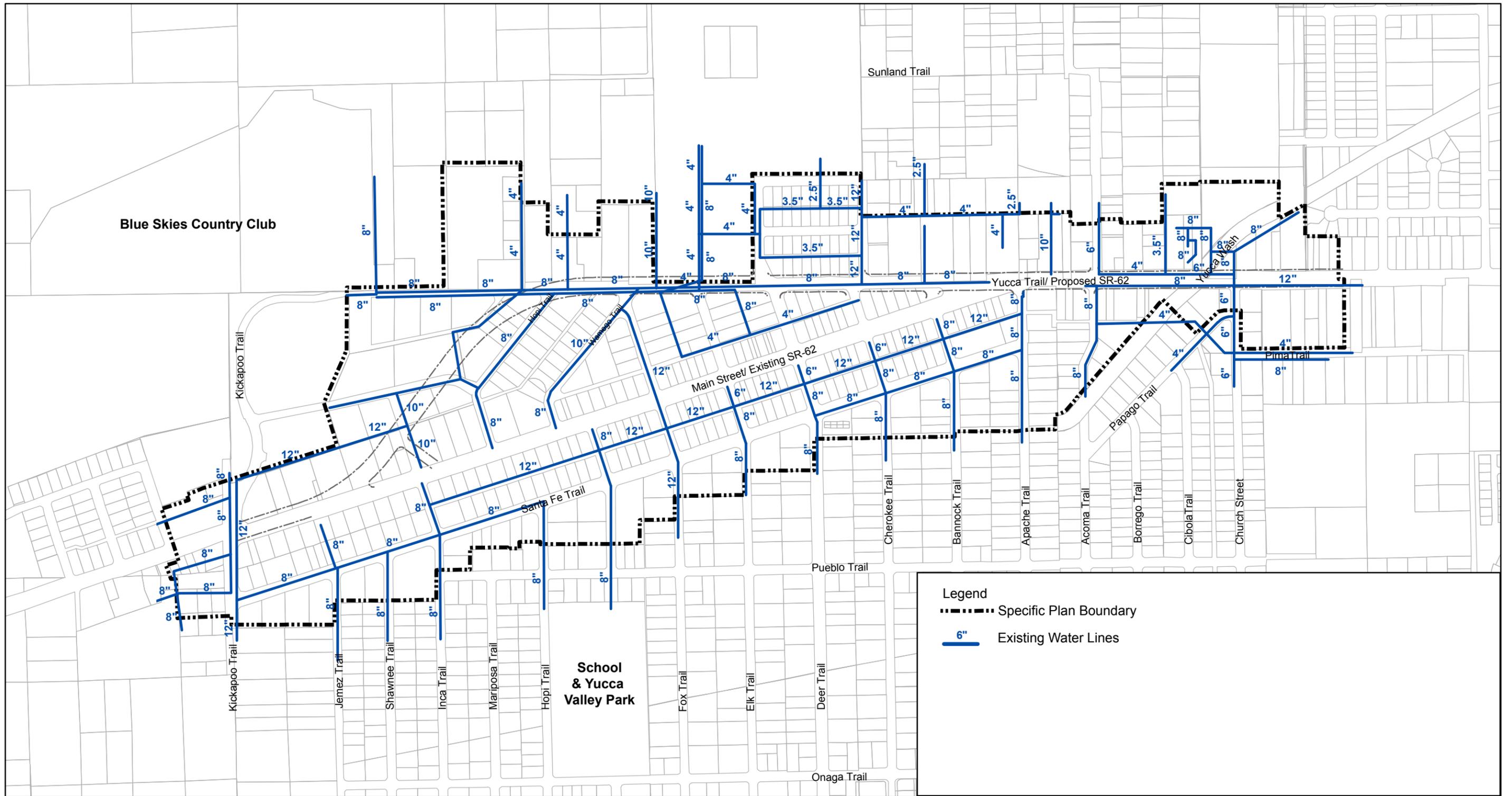
- Verify system pipeline upgrade diameters shown in Exhibit 4 using computer simulation of critical operating conditions;
- Periodically evaluate actual water consumption trends to verify the accuracy of City water usage factors for estimating future demands;
- Evaluate the feasibility of a regional wastewater collection system for high density developments, such as that proposed for the Revitalization Project; and
- Add the water system pipeline projects outlined here to the City's capital improvement program



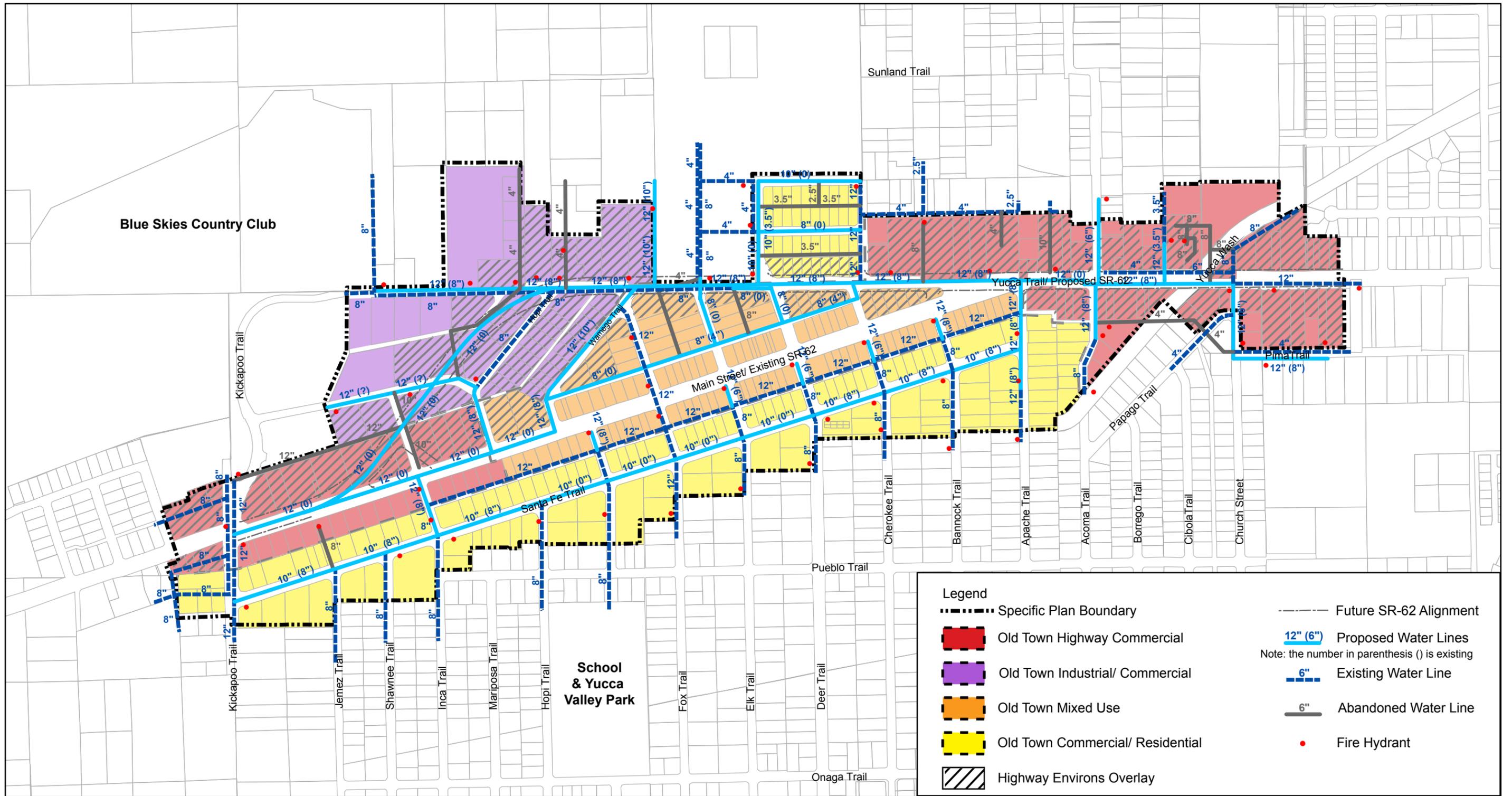
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Old Town Yucca Valley Specific Plan
Proposed Water Plan

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